

PLANNING COMMITTEE REPORT



ISLINGTON

Development Management Service
Planning and Development Division
Environment and Regeneration
Department
Islington Town Hall
Upper Street
LONDON N1 2UD

PLANNING COMMITTEE	AGENDA ITEM NO:	B3
Date: 18 July 2017		

Application number	P2017/1790/FUL
Application type	Full Planning Application
Ward	Clerkenwell
Listed building	No
Conservation area	No. Within 50m of New River (CA2) and Kings Cross CA (CA21).
Development Plan Context	Core Strategy Key Area: Kings Cross and Pentonville Road. Central Activities Zone (CAZ). Employment Growth Area (Offices) (Policies DM5.1, 5.2, 5.4) London Plan Vista 3A.1 & adjacent to Islington Local View LV8.
Licensing Implications	No
Site Address	York House, 207-221 Pentonville Road, London, N1 9UZ
Proposal	The addition of a single storey to the existing building (including lift overrun and 1.35m increase to parapet height) with plant enclosure above; a five storey front entrance extension; and a part 1, part 2 storey rear extension at upper and lower ground floor levels including the infilling of the rear undercroft area, to provide 1,005sqm (GIA) of additional office space (Use Class B1). Internal and external refurbishment works to provide ancillary gym and cafe areas; new fenestration; articulation to the south elevation; construction of a new front elevation light well; erection of new cycle and bin stores in refurbished service yard; hard and soft landscaping (including replacement of trees); roof level plant; and associated works.

Case Officer	Jan Slominski
Applicant	Esselco Office Properties Ltd
Agent	Lichfields

1. RECOMMENDATION

The Committee is asked to resolve to **GRANT** planning permission:

1. subject to the conditions set out in Appendix 1; and
2. conditional on the prior completion of a Deed of Planning Obligation made under section 106 of the Town and Country Planning Act 1990 securing the heads of terms as set out in Appendix 1.

2. SITE PLAN

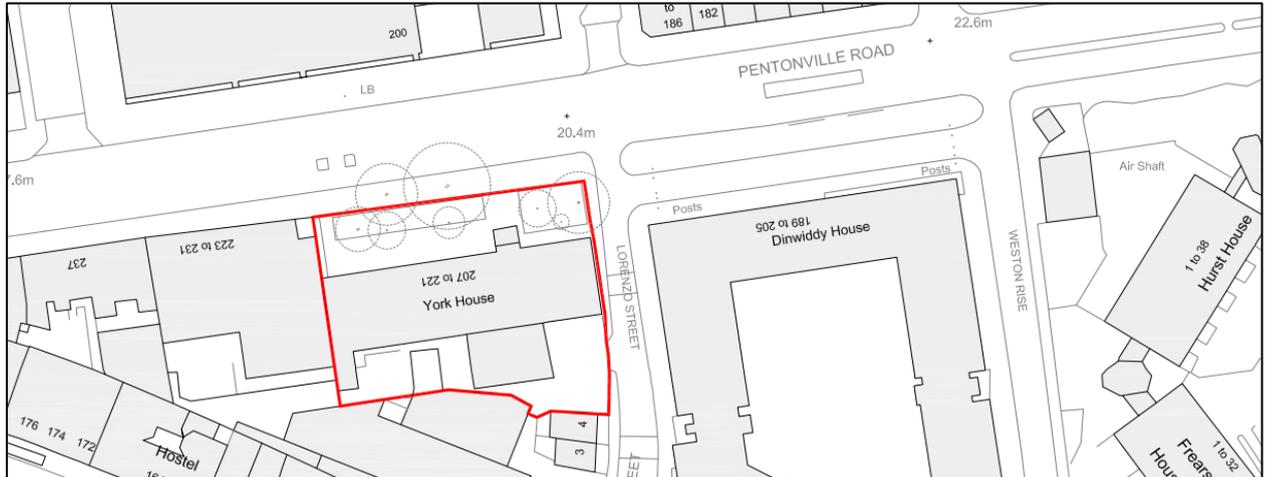


Fig 2.1 Site Plan. Application site outlined in red.

3. PHOTOS OF SITE AND SURROUNDINGS

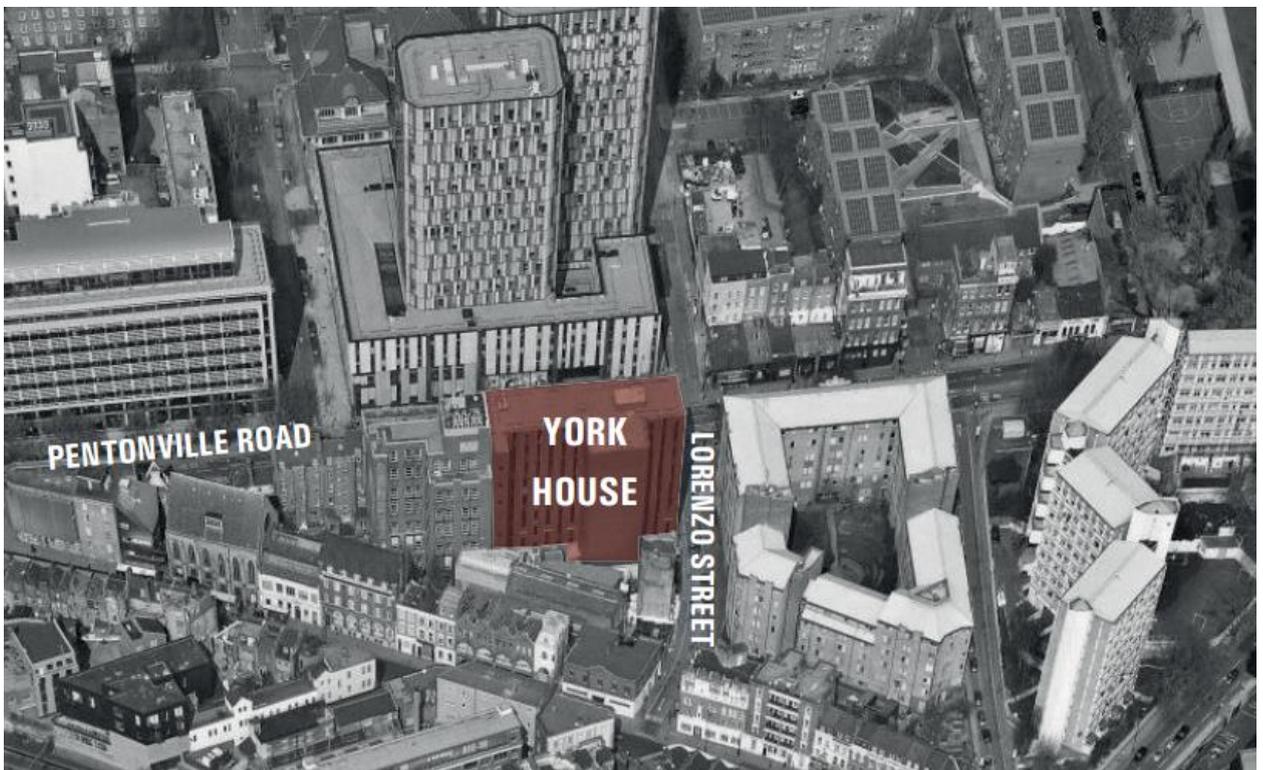


Fig 3.1 Aerial view of York House (looking North)

4. SUMMARY

- 4.1 The application site is an existing 7 storey brick building on the corner of Pentonville Road and Lorenzo Street, known as York House. The building design dates from the late 1970s, and it was in use as an office building, until recently when it became vacant.
- 4.2 The applicant is The Office Group, a provider of workspace for small and medium sized businesses. The proposal would refurbish and extend the existing building for continued use as an office building. Three extensions are proposed; a single storey (plus plant enclosure) roof extension, a 5 storey front extension including a new double height entrance foyer; and a 2 storey extension to the rear within the existing car park.
- 4.3 The site is within the Central Activities Zone, and an Employment Growth Area (Offices). There would be an uplift of 1,005sqm (GIA) of office (B1a) floorspace, 10% of which would be reserved as small units for small and micro enterprises, in accordance with the Council's affordable workspace policy. The proposal would not provide any housing on site, but it would make an equivalent financial contribution towards the provision of affordable housing elsewhere in the borough. The proposal would support continued economic growth and complies with the relevant land use priorities for its location.
- 4.4 The proposal would retain the existing building, but would extend it and upgrade the existing elevations. There would be a perforated metal clad extension at roof level with a zig-zag plan form, a 5 storey perforated brick extension to the front of the building, and a visually subordinate 2 storey extension to the rear. There would be a new brick parapet to "finish" the existing elevations, and new glazing including additional openings at ground level to provide a better street level relationship. There would also be new soft landscaping with new trees, in place of the concrete forecourt and 6 existing forecourt trees (which would be removed). Officers and the Council's Design Review Panel (DRP) consider that the proposal would be well designed, with sensitive upgrades to the retained building and striking modern extensions which would reinterpret the existing design language.
- 4.5 The existing building is not locally or statutorily listed and is not within a Conservation Area, but it is adjacent to locally protected view LV8 (views of the St Pancras Clock Tower along Pentonville Road), and adjacent to the Kings Cross Conservation Area (which in turn is adjacent to the New River Conservation Area). It is considered to have a neutral impact on the appreciation of the Clock Tower, and a neutral impact on the settings of both Conservation Areas.
- 4.6 The proposal would have very limited impacts on neighbouring amenity, would be designed to comply with the Council's policies on sustainability, and would avoid harm to the safe and efficient operation of the highway network.
- 4.7 The application has been considered with regard to the Development Plan and is considered to be a sustainable form of development, therefore approval is recommended subject to conditions and a Section 106 agreement to secure the necessary mitigation.

5. SITE AND SURROUNDINGS

- 5.1 The site is on the southern side of Pentonville Road, on the junction with Lorenzo Street. York House is a 7 storey, plus basement, brick building which dates from 1964. It is not of any particular historic or architectural interest but does have a Geoffrey Clarke sculpture on the east facade which could be considered of some artistic interest.

- 5.2 The site is not subject to a site allocation, but is within the Core Strategy Key Area: Kings Cross and Pentonville Road; the Central Activities Zone (CAZ); and an Employment Growth Area (Offices) (Policies DM5.1, 5.2, 5.4).
- 5.3 The site is not in a Conservation Area, but is adjacent to the King's Cross Conservation Area (directly to the south). The site is within wider setting area of London Plan protected viewing corridor 3A.1: Kenwood viewing gazebo to St Paul's Cathedral; and adjacent to Local Protected View LV8 (Pentonville Road to St Pancras Chambers) which runs along Pentonville Road to the north of the site. The site is not in an archaeological priority area.
- 5.4 The site slopes down to the south so the land at the south of the site is lower than Pentonville Road. Immediately to the south of the site (within the Conservation Area) are two Victorian properties (3 and 4 Lorenzo Street) which may have originally been part of a residential terrace. These buildings are not locally or statutorily listed.
- 5.5 Immediately to the west is Caledonian House which is a 5-7 storey office building.
- 5.6 To the east (across Lorenzo Street) is Dinwiddy House, a 6 storey brick building which provides student accommodation.
- 5.7 To the north (across Pentonville Road), 200 Pentonville Road is a mixed use development comprising a 5 storey podium with active frontage uses fronting Pentonville Road, and 2 towers housing student housing and residential units.
- 5.8 Pentonville Road is part of the Transport for London (TFL) Road network. Islington Council is the highways authority for Lorenzo Street. The site has a PTAL of 6B which is indicative of excellent accessibility by public transport. The site is served by 15 bus routes within 400m; and is within 725m of St Pancras and Kings Cross rail and underground stations with National Rail, Eurostar and underground services (Circle, Hammersmith & City, Metropolitan, Northern, Piccadilly and Victoria lines).
- 5.9 The site has a concrete paved forecourt adjacent to Pentonville Road, with 6 trees. The trees are not protected. There is no other green space on or within close proximity to the site.

6. PROPOSAL (IN DETAIL)

- 6.1 The proposal would retain the existing building on the site, but would extend and refurbish it, in order to increase the amount of floorspace from 5,238sqm (GIA) to 6,243 sqm; an uplift of 1,005sqm. The existing building is vacant, and its lawful use is within Use Class B1(a); offices. There would be no change of use but the works would include ancillary gym and cafe areas (spread throughout the building and not capable of being separated as separate units), and facilities including WCs, waste storage, and cycle storage.
- 6.2 The existing building has no particular architectural merit, but the proposal seeks to improve on its shortcomings by refurbishing the external elevations, and extending the building to enlarge the amount of internal floorspace. There would be three extensions; (1) a 5 storey extension to the front; (2) an additional storey (plus enclosed plant area) above the existing flat roof; and part 1, part 2 storey extensions at upper and lower ground floor levels including the infilling of the undercroft area beneath the rear wing.
- 6.3 The following images show the proposed development.

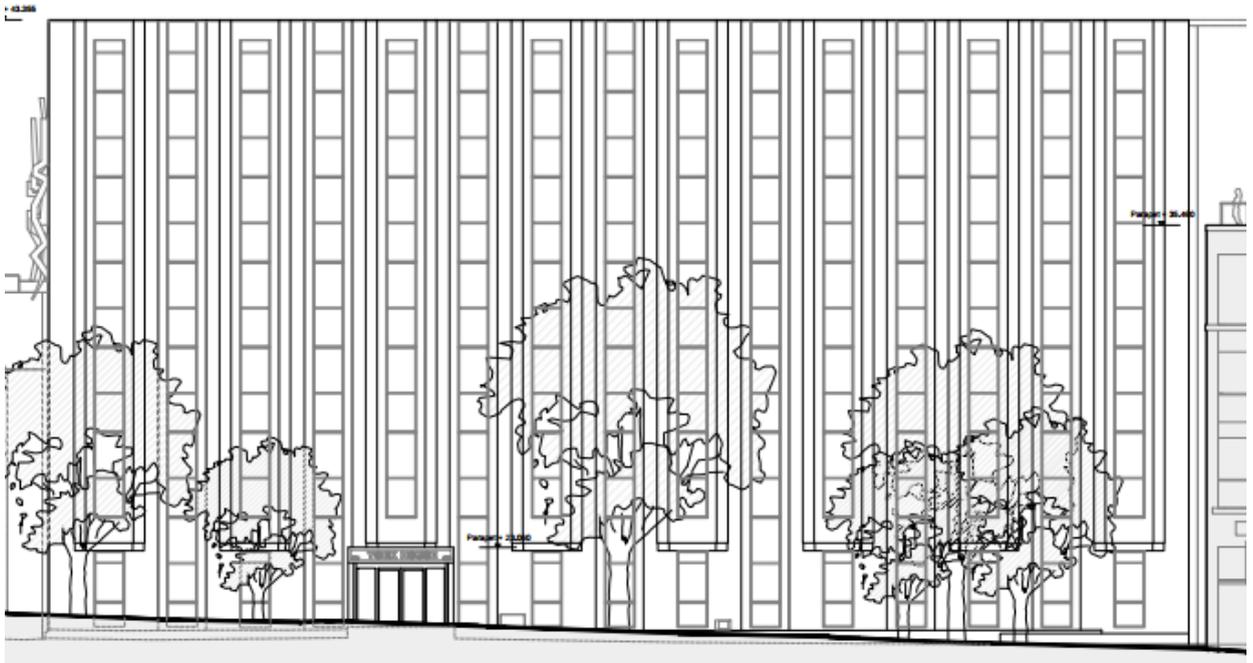


Fig 6.1 Existing Front Elevation

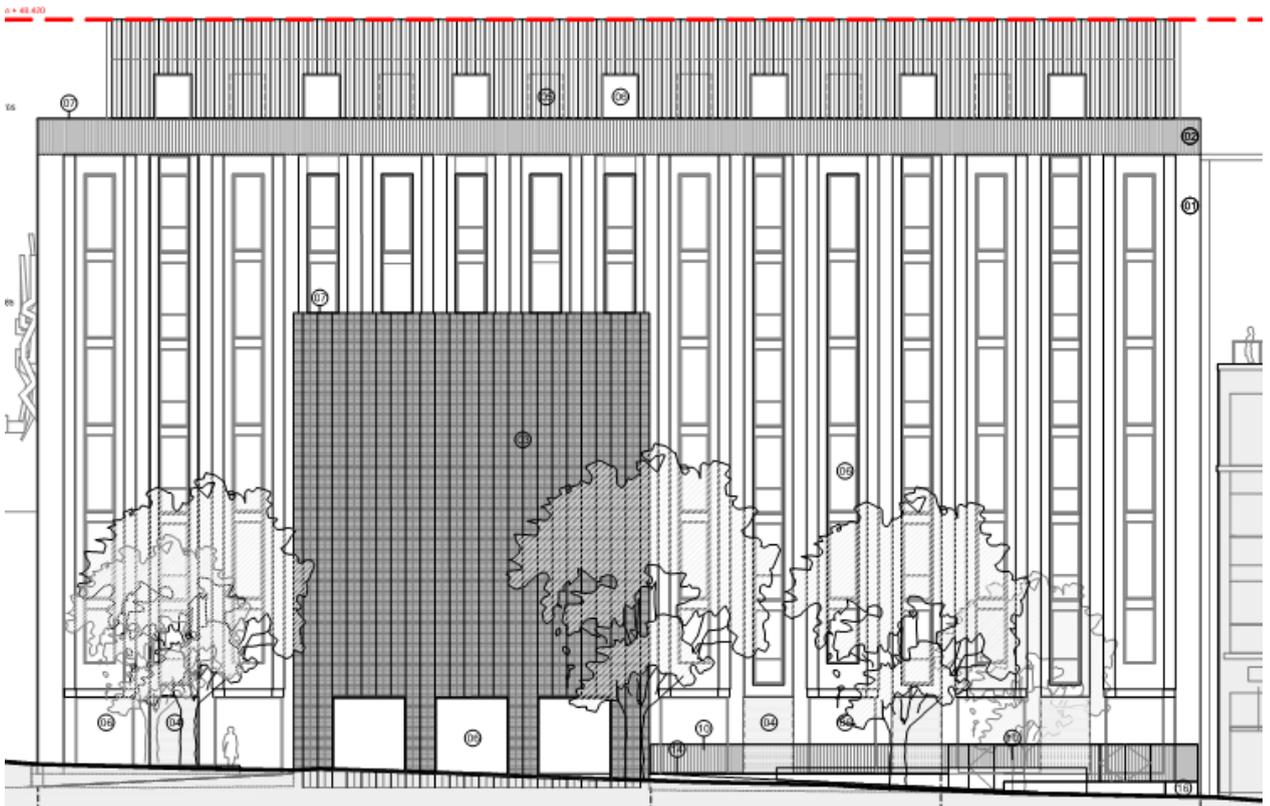


Fig 6.2 Proposed Front Elevation



Fig 6.3 Existing building (at the junction of Pentonville Road and Lorenzo Street)



Fig 6.4 Proposed building (at the junction of Pentonville Road and Lorenzo Street). Street trees to be retained, but omitted from image for clarity.



Fig 6.5 Proposed front extension (looking east towards Angel). Street trees to be retained, but omitted from image for clarity.

- 6.4 The 5 storey front extension would be built over part of the paved forecourt, and would bring the front elevation in line with the adjacent Caledonian House (which is also 5 storeys high). This would be a timber framed, fully glazed extension, which would be externally clad by brick lattice walls using similar brick to the existing building. The external lattice brick walls would have a zig zag plan which would allow limited views through into the building. The extension would provide a new double height entrance lobby for the building, and three floors of office/meeting room space above. The design and access statement points out that the building is generally approached from the east and west, and consequently new entrances are proposed on the east and west sides of the proposed extension as these would be highly visible on the approaches from Angel and Kings Cross. The new entrances would be approached via the retained and re-landscaped forecourt areas. There are 2 street trees on the pavement which would be retained, and 6 smaller trees on the forecourt which would be removed (with new trees and planting) as part of the landscaping scheme.
- 6.5 At roof level, the design and access statement points out that the existing windows rise to parapet level with no capping or parapet, giving the building an “unfinished” feel. The proposal would introduce a 1.35m high parapet in order to “finish” the existing elevations, and would then add a single storey extension with an integrated plant enclosure above. The extension would be 5.0m high (3.65m above the parapet) including the plant enclosure (which would also include the lift overrun). The site slopes so the height relative to ground level varies. The proposal would increase the total building height from 25.0m (measured from the average ground floor level) to 30.0m. The site slopes down towards the south (rear), such that the highest part of the proposal (the top of the plant enclosure) would be 32.2m above adjacent ground level.
- 6.6 The roof level extension would partially provide office space, and partially an integrated plant enclosure, designed as one single structure. There would be an external roof terrace at the rear, set behind the parapet.

- 6.7 At ground and lower ground floor, there would be a part 1 storey, part 2 storey, metal and glass rear extension including two roof terraces accessed from building, and the infilling of the existing undercroft area.
- 6.8 There would also be new windows across the whole building, with glazing replacing the existing vertical spandrel panels between windows; and new glazing on the front elevation separated by perforated brick piers (with glass bricks).
- 6.9 There would be external plant which would be mostly located within the top floor plant area (with the exception of an extract vent into the servicing yard at first floor level), cycle and bin storage in the service yard, and soft and hard landscaping to the front courtyard.

Revisions to the proposal

- 6.10 The proposal has been revised since the original submission. As originally proposed, there would have been a 2 storey roof level extension, which would have been 5.7m high to eaves level (4.35m above the increased parapet) in addition to a further 0.8m high lift overrun; this would have increased the total building height from 25.0m (measured from the average ground floor level) to 31.5m. Feedback was provided by officers stating that this would be contrary to the Council's tall building policies (CS9, DM2.1, and BC9). The proposal was subsequently reduced in height by 1.5m, including the plant enclosure and integrated lift overrun. As noted in the "Design and Conservation" section of this report, the reduced height would now comply with the tall building policies (it would be no higher than 30m when measured in accordance with the Council's guidance). As a result, the proposed uplift in office floorspace has been reduced from the originally proposed 1,228sqm, to 1,005sqm.

7. RELEVANT HISTORY

- 7.1 The following history is considered most relevant to the application site.

Planning History

- 7.2 P2014/4370/FUL Retention of temporary change of use of ground floor from B1 to retain Use Class D1 meeting rooms and canteen associated with a church open from 7:00am-11:00pm daily for a further period of three years. Approved 07/01/2015.
- 7.3 P121749 Temporary change of use of ground floor from B1 office accommodation to offices and meeting rooms associated with a church (Use Class D1). Approved 18/10/2012
- 7.4 Officers note that the approved temporary D1 use was permitted until 07/10/2018, but that this use has now ceased and the building is vacant. Condition 4 of permission P2014/4370/FUL states that "*...on the cessation of the temporary use hereby granted the building and land shall revert to the B1 use/purpose for which it was normally used prior to the grant of this planning permission.*" Therefore the temporary use has ceased and although the building is vacant the lawful use of the building has reverted to its previous B1 use.
- 7.5 Officers have also reviewed further planning history relating to signage, advertisements, doors and plant; none which is particularly relevant to the current proposal.

Pre application advice

- 7.6 Prior to submission of the application, a request for pre-application advice was submitted to the Council. The advice given was that the priority land use on the site is office use, and that a proportion should be secured as affordable workspace. The proposed roof extension should be one simple form, avoiding multiple setbacks and visual clutter; the acceptable height should be determined by a townscape assessment and Accurate Visual Representations (AVRs). Officers advised that the front extension would be acceptable in principle, but this was presented with no fenestration on the front elevation, and some activation is needed on this elevation. The principle of refurbishing and extending the building for intensified office floorspace was supported in principle, subject to good design and compliance with other aspects of the development plan (servicing, transport, sustainability, neighbour amenity etc.)
- 7.7 Following consideration of the advice given, and in preparation for the submission of a planning application, further follow-up advice was given relating to energy, drainage, transport, servicing and deliveries.

Design Review Panel (1)

- 7.8 Islington's Design Review Panel considered the proposal on 16th January 2017.
- 7.9 The Panel's pre-application stage written comments (issued on 6th February 2017) are appended as Appendix 3.

Design Review Panel (2)

- 7.10 Islington's Design Review Panel considered an amended version of the proposal on Tuesday 13th June 2017.
- 7.11 The Panel's pre-application stage written comments (issued on 30th June 2017) are appended as Appendix 4, and the relevant design comments made are as follows.
- 7.12 "The Panel welcomed the opportunity to comment on the scheme for a second time and remained supportive of the design team's approach to the site. Comments made by panel members predominantly related to fine-tuning the design of the new additions to the building, notwithstanding the contrasting views expressed with regard to the impact of the extra height and bulk at roof level." (*Officer comment: The proposal is generally considered acceptable in design terms, subject to appropriate materials and details which will be required by condition 3*).
- 7.13 "Front Elevation: The Panel commended the design strength and abstract quality of the brick lattice structure, and welcomed the positive use of redundant space at the front of the building which represents an enhancement of the public realm. Panel members expressed a range of differing views on how best to reconcile the desire to give the street fronting elevation more of an active frontage, incorporating human-scale elements, without compromising the concept and overall composition. Whilst some panel members considered the introduction of three large openings to be a successful response, others were mindful that these openings would make the transparent nature of the lattice brickwork more difficult to appreciate. In light of the diversity of the comments, the Panel felt that further experimentation could result in a solution that satisfied all concerns without compromising the design. This could be the creation of a more transparent lattice which utilized larger or doubled spaced openings in the lattice pattern, rather than punched openings or portals. The Panel also felt that a more convincing argument needed to be developed, demonstrating that what is ultimately proposed is the correct

approach, clearly showing how the chosen arrangement had been settled upon and why others had been ruled out.” *(Officer comment: There were differing views on the fenestration on the front elevation, with some panel members more supportive and others expressing no overall view. No objection was made by the panel to this treatment. When responding to the officer and DRP feedback, the architect made amendments to other elements of the scheme but advised that the introduction of taller openings on the front elevation would result in a “grand” appearance which would undermine the prevailing design language and hierarchy of entrances and openings. Officers consider that this design approach would offer sufficient human scale activation to the front elevation, and that the design is acceptable.)*

7.14 “Roof addition: Panel members welcomed the revision of this element in accordance with previous advice – in that it has been simplified so that it reads as a single entity. The use of metal cladding in place of brick was also welcomed by the Panel, as it is considered to help to disassociate the extension from the main body of the building, appearing as a much lighter weight structure. However, some Panel members raised significant concern over the extra bulk now present in long views down Pentonville Road and from within the Conservation Area. Nevertheless, the majority of the Panel considered the height not unacceptable, and perhaps necessary in enabling the quality of the design to be recognized and noticed. Although the two points of view expressed are somewhat conflicting, the Panel regarded them both as equally valid considerations. Consequently, in the view of the Panel, the acceptability of the size of the roof addition will ultimately rest upon the quality of the design – great emphasis was therefore placed on the importance of showing that much care and detail has gone into the final design, in addition to a greater range of verified views, to ensure that decision takers are able to make as informed a judgement as possible. The Panel suggested that an in situ mock up sample panel of the attic storey would be of benefit in assessing the design quality.” *(Officer comment: The proposal has been reduced in height by 1.5m since the second DRP meeting, with the lift overrun incorporated into the plant enclosure; revised verified views were submitted to the Council and considered by officers; and condition 3 is recommended to secure high quality materials and detailing.)*

7.15 “Rear elevation: The Panel discussed the proposal of introducing some form of public art to animate the large blank southern elevation and generally supported the idea which they believed was more in keeping with the existing character of the building than introducing an ad hoc window pattern. However, the Panel considered that a degree of control over what is put there to be absolutely essential, stating that the acceptability of this intervention rests entirely upon the quality and contextual appropriateness of the artwork. Panel members suggested that it should be of the same stature as the existing Geoffrey Clarke sculpture and perhaps similar in nature (either abstract, sculptural or both), which would help to provide an element of control. Some Panel members suggested that the Design Review Panel could have further input in drawing up selection criteria or judging entries.” *(Officer comment: officers agree that the large blank south elevation should be animated to balance the additional bulk of the roof extension, and that control over the quality and contextual appropriateness of this is key to its acceptability. The elevation in question is a rear secondary elevation and there is already a George Clarke sculpture on the east elevation, so there is no requirement for piece of art on this elevation. The panel’s comments focussed on the type of art which would be appropriate, rather than whether art in itself is required, and also called for something which is controlled and contextually appropriate. The proposed rear elevation has been*

updated to show some indicative architectural treatment, and condition 3 is recommended to secure details of articulation to the existing south elevation in order to offset the impact of the additional bulk resulting from the roof extension.)

8. CONSULTATION

Procedural Matters

- 8.1 Letters were sent to occupants of 968 adjoining and nearby properties. Site notices and a press notice were published on 18/05/2017. The public consultation of the application therefore expired on 08/06/2017; however it is the Council's practice to continue to consider representations made up until the date of a decision.

Public Consultation

- 8.2 At the time of writing, no objections were received from neighbours. It is noted that the majority of neighbours are either businesses, or occupants of the large amount of adjacent student accommodation. Officers also note that the submitted planning statement states that only one person attended the pre-application consultation event.

- 8.3 One comment was received from a business on Kings Cross Road, neither objecting nor supporting the proposal. The response stated that the daylight and sunlight assessment only takes account of residential properties, and no survey was carried out in relation to the light interference affecting local businesses. *(Officer comment: The Council's planning policies protect neighbour amenity and there is no specific protection of sunlight and daylight to business properties. The proposal is to the north of Kings Cross Road such that business premises on Kings Cross Road would not be affected by loss of direct sunlight. In any case, the distance from the premises on Kings Cross Road is sufficient that any loss of daylight is likely to be minor; and there is another building (1-2 Lorenzo Street) between the site and Kings Cross Road which offers a greater barrier to daylight than the proposal.)*

External Consultees

- 8.4 London Underground (Infrastructure Protection): No objection.
- 8.5 Historic England: No objection. The application should be determined in accordance with national and local policies, and on the basis of the Council's own specialist advice.
- 8.6 Thames Water: No objection.
- 8.7 Transport for London: No objection. The proposed cycle storage should be secured by condition. Off-street servicing is supported, but the delivery and servicing plan should manage trips to minimise the overall number, and these should be limited to between 10am and 4pm to avoid peak times. The construction management plan should take account of the planned Kings Cross Gyratory works. *(Officer comment: A draft construction management plan was submitted with the application, and condition 9 requires submission of a detailed application stage plan including reference to the Kings Cross Gyratory works. Tree protection measures were submitted with the application, and are recommended to be secured by condition 14)*

Internal Consultees

- 8.8 Environmental Health: No objection subject to a condition requiring details of any proposed flues or extract systems. *(Officer comment: details are to be secured by condition 17).*
- 8.9 Acoustics and Pollution: No objection subject to conditions requiring further details in relation to land contamination, construction management, and plant noise limits. *(Officer comment: details are to be secured by conditions 19, 9 and 5 respectively).*
- 8.10 Energy Officer: Concerns were initially raised, stating that a BREEAM pre-assessment should be provided, a carbon offset contribution is required for both the new build and refurbishment elements of the proposal, details of mechanical ventilation are required, investigation of a shared energy network should be undertaken, and roof level PV arrays should be considered. *(Officer comment: a revised sustainable design and construction report was submitted, setting out measures for BREEAM compliance to be secured by condition 18, a full carbon offset contribution of £231,702 to be secured by the s.106 agreement, an energy strategy secured by condition 20, details of roof level PV panels and an air source heat pump secured by condition 11, and consideration of DEN connection to be secured by the s.106 agreement).*
- 8.11 Inclusive Design Officer: No objection. Additional details were however requested in relation to the location of on-street disabled parking bays; accessible cycle parking; mobility scooter charging; step free access to the rear; emergency provision for disabled people; and disabled WCs on each floor. *(Officer comment: the plans have been amended to include a disabled WC on each floor, and the other details are either to be secured by conditions: cycle and scooter facilities secured by condition 21; accessible circulation and access by condition 4, and the on-street disabled parking bay to be secured by the s.106 agreement.)*
- 8.12 Highways Officer: No objection. A Construction Management Plan would be required. *(Officer comment: details are to be secured by condition 9).*
- 8.13 Design and Conservation Officer: No objection. As previously proposed, the two storey roof extension was considered overly dominant, particularly when viewed from long views down Pentonville Road, towards King's Cross and the St Pancras spire. The reduction in height by 1.5m (from the original proposal) is welcomed and would overcome the previous concerns such that no objection is raised. The design of the roof addition is considered high quality, and appropriate to design of the building. The addition of art to the blank rear (south) elevation wall is inappropriate as there is already a Geoffrey Clarke sculpture to the flank elevation fronting Lorenzo Street and an additional piece of art would undermine it. A better, more architectural solution should be found. The front extension is acceptable in principle, and includes three ground level windows on the front elevation in response to officer concerns at pre-application stage about this being a blank elevation. This is considered a "stiff" response, but would improve the extension's relationship with the street and introduce some human scale to this front elevation. Overall, the proposal is considered acceptable subject to a condition allowing officers to secure high quality materials and details. *(Officer comment: details of articulation to the rear elevation are to be secured by part k of condition 3, and materials and detailing are also to be secured by condition 3).*
- 8.14 Tree Officer: No objection subject to conditions, but advised that the removal of the trees within the forecourt area should be mitigated by replacement trees with sufficient canopy cover, and the three trees proposed are unlikely to be sufficiently large to achieve this.

The tree pits required will be large and require design prior to other construction works taking place, so a pre-commencement condition is recommended. (*Officer comment: tree protection measures are to be secured by condition 14 and notwithstanding the submitted documents, details of new trees and tree pits are to be secured by condition 6 which was drafted jointly with the tree officer*).

9. RELEVANT POLICIES AND LEGISLATION

9.1 Details of all relevant policies and guidance notes are attached in Appendix 2. This report considers the proposal against the following development plan documents.

National Guidance

9.2 The National Planning Policy Framework (NPPF) 2012 is a material consideration which seeks to secure positive growth in a way that effectively balances economic, environmental and social progress for this and future generations. Since March 2014 planning practice guidance for England has been published online.

Development Plan

9.3 The Development Plan is comprised of the London Plan 2016 (amended), Islington's Core Strategy 2011, Development Management Policies 2013, Finsbury Local Plan 2013 and Site Allocations 2013. The relevant Development Plan policies are listed in Appendix 2.

Designations

9.4 The site has the following designations under the London Plan 2016 (amended), Islington Core Strategy 2011, Development Management Policies 2013, Finsbury Local Plan 2013 and Site Allocations June 2013:

9.5 Core Strategy Key Area: Kings Cross and Pentonville Road

9.6 Central Activities Zone (CAZ)

9.7 Employment Growth Area (Offices) (Policies DM5.1, 5.2, 5.4)

Supplementary Planning Guidance (SPG) / Document (SPD)

9.8 The relevant SPGs and/or SPDs are listed in Appendix 2.

Environmental Impact Assessment (EIA)

9.9 No request for an Environmental Impact Assessment (EIA) scoping opinion was submitted, however the site is significantly less than 1 hectare in size and it is not in a sensitive area as defined by the Infrastructure Planning (Environmental Impact Assessment) Regulations (2017). As such the proposal is not considered to fall within the development categories of Schedule 1 or 2 of the EIA Regulations and an EIA is not considered necessary.

10. ASSESSMENT

10.1 The main issues for consideration are:

- Land-Use
- Design and Conservation
- Neighbouring Amenity

- Accessibility
- Landscaping and Trees
- Sustainability, Energy Efficiency and Renewable Energy
- Highways and transportation

Land Use

Office Use

- 10.2 The existing building is vacant but is understood to have been last in use for offices (Use Class B1a). The proposal would not change the use of the building, but would refurbish and extend it for use as offices, with ancillary facilities (a café area, a basement gym/studio area, and outdoor amenity areas).
- 10.3 The site is located in the Central Activities Zone (CAZ) and an Employment Growth Area. Policy CS13 protects existing employment space within the CAZ against change of use to non-business uses, and supports intensification of offices within this location (supported by Policies CS6, DM5.1, DM5.2, and DM5.4).
- 10.4 Policy CS6 (King's Cross) relates to the area surrounding the site, and expects development within this location to accommodate approximately 3,200 new jobs over the life of the plan, and Pentonville Road is identified as a principal location for office-led mixed use growth. Policies CS13, DM5.1 and DM5.2 further seek to safeguard and enhance existing business spaces in the Borough.
- 10.5 Due to its location within an Employment Growth Area, the site is considered to be capable of accommodating a diverse range of businesses and enterprises. In Employment Growth areas, the Council encourage the intensification, renewal and modernisation of existing business floorspace including the reuse of otherwise surplus large office spaces for smaller units.
- 10.6 The Council's recent Employment Land Study (2016) identifies the Pentonville Road corridor as one of the keys to delivering this, recommending that:
- 10.7 *"Site assembly for Grade A office space in the CAZ should be a priority, to maintain and enhance the area's role in supporting London's strategic business role. The arc stretching between Shoreditch and King's Cross, mainly along Pentonville Road/City Road should form the focus of this priority, alongside delivering additional, lower specification office space."*
- 10.8 The need to deliver office space has become even more pressing given the large scale loss of B1a space to permitted development rights in Islington, almost 10,000sqm since 2013. The Employment Land Study identifies a shortfall in the current pipeline of office supply when compared to projected needs of around 400,000sqm over 2014-2036 in order to meet forecast demand and allow for 8% frictional vacancy. The study states the spatial growth pattern of London means that the southern end of Islington within the CAZ will be the most attractive location for office occupiers, particularly for the professional, scientific and technical services activities which have been expanding rapidly in London and which are projected to experience further substantial growth.
- 10.9 This up-to-date evidence reinforces the above policies and underpins the policy priority for office led development in this location.

- 10.10 The policy priority in land use terms is therefore to secure uplift and intensification of employment floorspace in order to support the borough's potential for jobs growth.
- 10.11 The proposal would result in improvements to the attractiveness and quality of the existing office space, and a significant increase in employment floorspace in the form of an additional 1,005 sqm (GIA) of B1 office space, (resulting in a total of 6243sqm GIA).
- 10.12 The proposed uplift in office floorspace would be supported by the Council's policies.

Affordable Workspace

- 10.13 Policy DM5.4 addresses the affordability and suitability of employment floorspace for small and micro enterprises. Part A states that "Within EGAs and town centres, major development proposals for employment floorspace must incorporate an appropriate amount of affordable workspace and/or workspace suitable for occupation by micro and small enterprises."
- 10.14 As set out in paragraph 5.25 of the DMP, the figure of 5% of gross floorspace should be taken as the starting point for provision. The space should either be provided as separate small units for SME businesses (affordable by virtue of their size) or let to the council as Head Leaseholder at a peppercorn rent for at least 10 years; (in such cases the council will then engage with approved workspace providers to manage the space and ensure it is occupied by target sectors).
- 10.15 The applicant trades as The Office Group, which is a provider of workspaces for small and medium enterprises. The internal spaces within the building would be accessed by a new double height entrance, and the internal layout would allow a mix of sizes of office units, shown on the plans as ranging in size from 20 to 150sqm with shared access to meeting rooms, breakout spaces, and ancillary facilities. The Office Group leases individual desks, drop-in lounge memberships, small offices (for 2 to 400 people), and separately bookable meeting rooms. The proposed layout and arrangement of spaces managed in this way would effectively allow 100% of the floorspace to be used by SME businesses (with the option to expand and take additional grow-on space). This is welcomed, but would not in itself result in compliance with the Council's affordable workspace policy. In any case the planning permission applies to the land, not the occupier, and there is no practical purpose why the building could not be used by one large business in future. In order to secure the policy requirement and ensure that SME workspace is secured notwithstanding the operator, the applicant has suggested that 10% of the uplift in floorspace (100.5 sqm) is secured in perpetuity as small units, suitable for SME businesses. This would exceed the guidance set out in paragraph 5.25 of the DMP thus would be considered to mitigate the impacts of the development (in terms of wider displacement of SME workspaces by large office developments), and would be secured by condition 23.

Housing in the Central Activities Zone

- 10.16 London Plan (LP) Policy 2.10 recognises the 'mixed' nature of much of the CAZ and seeks to enhance and promote the unique international, national and London wide role of the CAZ through the promotion of a range of mixed uses, and LP Policy 2.11 indicates that boroughs should ensure that development proposals to increase office space within the CAZ incorporate a mix of uses including housing, subject to compliance with other policies. LP Policy 4.3 states that within the CAZ increases in office floorspace should provide for a mix of uses including housing, unless such a mix would demonstrably

conflict with other policies in this plan. In the supporting text to this policy, paragraph 4.15 states that London's economic growth depends heavily on an efficient labour market and this in turn requires adequate housing provision to sustain it.

- 10.17 In order to achieve the requirement for housing to be delivered alongside business uses, Part E of Policy DM5.1 states that within the CAZ, major development proposals that would result in a net increase in office floorspace should incorporate housing, and where less than 20% of the total net increase in office floorspace is not housing, an equivalent contribution will be sought for the provision of housing off-site. The supporting text (paragraph 5.9) states that where it is not appropriate for housing to be provided on site, an equivalent financial contribution will be sought for the development of affordable housing off-site by the council (para. 5.9).
- 10.18 Given the policy priority for office floorspace in this location, and the weight of evidence provided by the Employment Land Study, no objection is raised to the omission of residential units. However, an equivalent financial contribution towards the provision of off-site affordable housing is to be secured by the s.106 agreement. Based on 20% of the uplift, the equivalent amount of housing would be 245.6sqm, and (based on the calculation set out in the Planning Obligations SPD) would result in a contribution of £160,800 towards affordable housing.

Delivery and Infrastructure

- 10.19 Policy CS 18 (Delivery and infrastructure) states that the council will work with its partners to deliver the infrastructure required to support development, and will require contributions from new development to ensure that the infrastructure needs are provided for and that the impacts of the development are mitigated. The proposed development would be subject to s.106 obligations to ensure that appropriate education and training opportunities arise from the development, which would require a local employment and training contribution and a construction training placement during the construction period. Further details of planning obligations are set out in the relevant sections of this report, and as a full list in Appendix 1.
- 10.20 The Mayor of London's Community Infrastructure Levy (CIL) is applied to developments across London to help fund the delivery of the Crossrail underground railway line. Within the Central Activities Zone there is a further requirement for certain proposed developments to help fund Crossrail, set out within London Plan Policies 6.5, 8.2 and 8.3, and detailed by the Islington Planning Obligations SPD and GLA Crossrail Funding SPG. The requirement is for a financial contribution charged at £140 per square metres of office floorspace, which is offset against the Mayoral CIL payment. The proposed development would require a payment of £140,700 towards Crossrail, to be secured by the s.106 agreement (see the "Planning Obligations" section of this report).

Land Use Summary

- 10.21 The proposed development would comply with the overarching land use policy, would deliver improved and additional office floorspace including provision for small and micro enterprises, and would provide a policy compliant financial contribution towards affordable housing elsewhere in the borough. The proposed intensification of office floorspace would therefore be acceptable in principle, subject to compliance with the other development plan policies.

Design and Heritage

- 10.22 The existing 7 storey building is a late 1970s design with dark brick facades, protruding bay windows offering a vertical emphasis, and blank side/rear elevations. The entrance on Pentonville Road is modest in comparison and set back behind a concrete paved area, with limited street level activation.
- 10.23 The submitted design and access statement highlights that the building is a solid structure, with high quality materials and interesting detailing, but also notes its shortcomings. Rather than replace the building, the proposal would retain the positive aspects of the existing design, address some of its shortcomings, and extend and modernise the building to create redesigned and additional office floorspace.
- 10.24 The NPPF Core Planning Principles (Paragraph 17) include that planning should always seek to secure high quality design, and paragraph 56 states that good design is a key aspect of sustainable development and indivisible from good planning.
- 10.25 London Plan Policies 7.4, 7.5, 7.6, 7.7 and 7.11 require buildings to make a positive contribution to their public realm and streetscape, to be of the highest architectural quality and to be of proportions, composition, scale and design which enhances and appropriately defines the public realm. Buildings should not cause unacceptable harm to surrounding amenity and should make the public realm comprehensible at a human scale, particularly at ground level. These policies are supported locally by Islington Policies CS8 and CS9 which encourage traditional street patterns and sympathetic building designs, and DM2.1 and DM2.3 which require development to be of high quality contextual design and to conserve or enhance the conservation area's significance (where the proposal would affect the setting of a conservation area).
- 10.26 London Plan (2016) Policy 7.7 states that tall buildings should be limited to designated sites, relate well to the form and character of surrounding buildings and contribute to improving the legibility and permeability of the site and wider area.

Height, Scale and Massing

- 10.27 Islington Policy CS9 states that tall buildings are generally inappropriate to Islington's predominantly medium to low level character, therefore proposals for new tall buildings will not be supported. Policy DM2.1 states that the only locations in Islington where tall buildings may be suitable are set out in the Finsbury Local Plan, and the suitable locations set out in Finsbury Local Plan Policy BC9 exclude the application site. Development Management Policies paragraph 2.18 defines tall buildings in Islington as those over 30m in height, and paragraph 11.2.7 in the Finsbury Local Plan provides clarification on the application of the Council's policies in relation to height. This states that the height of a proposed building:
- 10.28 *"should be taken to mean the distance between the average ground level of the site and the highest point of the building or structure. The actual and perceived height of a building relates to a range of factors: for example, variation in floor-to-ceiling heights ... architectural treatments and features (particularly at ground floor and roof level), and the site's prominence (either in built or topographical terms)."*
- 10.29 At roof level, the proposal would introduce a 1.35m high parapet which would raise the height of the existing elevations. The proposal would also add a roof level extension which would be 5.0m high (3.65m above the parapet) and would take the form of a single storey of additional accommodation, with an integrated, enclosed plant area above. All

external plant and the lift overrun would be contained within this area, so no parts of the proposal would be taller than the plant screen.

- 10.30 The roof addition would be set back 1.4m from the front elevation, 1.4m from the rear elevation, 2.6m from the (most prominent) east elevation and 0.9m from the (least prominent) west elevation. This would be faced in perforated metal cladding, with a zig-zag profile. There would be a roof terrace on the rear elevation, which would be set behind the parapet eliminating the need for balustrading.
- 10.31 The site slopes steeply down to the south, away from Pentonville Road, thus the height of the building varies in relation to the ground level. On its most prominent elevation (north elevation fronting Pentonville Road), the proposed building would be between 28.1m and 29.3m high when measured from the adjacent ground level; this is under the 30m height threshold (over which it would be considered a tall building). However the site slopes to the south west, and when measured from the lowest adjacent ground level, at the least prominent corner of the building (within the rear parking area in the south west corner of the site), the highest part of the building (the top of the plant enclosure) would be 32.2 m above ground level. The difference in the building's height arises from the sloping ground level. When considered against the above guidance within paragraph 11.2.7 of the Finsbury Local Plan, the mean ground level of the building is 18.4m AOD, and the highest part of the building would be 48.4m AOD; the difference between these heights is 30m. The building is therefore not considered to be a building which is over 30m in height in accordance with the Council's measurement guidance (as set out in paragraph 11.2.7 in the Finsbury Local Plan) and would not be contrary to the tall buildings policies. Notably, the part of the building which is highest above ground level is the least prominent corner (and this arises because the ground level slopes, rather than variation in the building's height); and when viewed from its most prominent elevation (the Pentonville Road elevation) the highest part of the proposal would be lower than 30m (at 28.1m to 29.3m high).
- 10.32 Officers note that new buildings in this location which are over 30m tall (notwithstanding the Council's guidance to its own tall buildings policies) are subject to referral to the Mayor of London. The proposal is an extension to an existing building, and although it would be over 30m tall in some locations, the additional height arising from the extension is less than 15m, thus the proposal would fall outside the criteria for referral to the Mayor.
- 10.33 The proposal was considered by the design officer and the Design Review Panel, and it was considered that the roof addition would be visually subordinate to the existing building, and would avoid a top heavy or disproportionate appearance. In terms of its impact on the appearance of the building, the additional height is considered acceptable.
- 10.34 In addition to the roof extension, a new extension is proposed to the front of the building. Currently, the building sits at the end of a row to the west, of 4-5 storey buildings on Pentonville road (the adjacent Caledonian House has 5 storeys plus an additional 1-2 storeys which are significantly set-back from the front elevation). These buildings follow an established building line close to the pavement edge. To the east of the site the character changes and the buildings are generally larger with more generous settings. York House is 7 storeys high, with a substantial forecourt, which is currently paved with concrete and makes no positive contribution to its setting. The site therefore sits at a transition between the 4-5 buildings to the west which abut the pavement edge, and the larger buildings to the east with generous settings. The proposed front extension would

be 5 storeys high and would bring the perceived eaves height down and push the building line forward, similar to the rest of the adjacent row to the west. The building is 43.5m wide and the extension would be 13.3m wide (less than a third of the building's total width) and two storeys lower than the existing eaves height. The size, positioning and massing of the extension has been considered by the DRP and the Council's design officers and is considered to improve the massing of the proposal, and to better articulate the transition between the separate character areas to the east and west of the site. The retained area of forecourt would be re-landscaped with new (replacement) trees to soften the appearance of the building.

- 10.35 The proposal would also include a 1-2 storey extension to the rear, including the infilling of an undercroft area within the parking area. This would not be visible from the public realm and would be modest in scale. There would be a roof terrace above for workers at the extended office building. This extension is considered to have a minor impact on the scale of the building and no objection is raised to its height, scale or massing.

Views and heritage impacts

- 10.36 The Planning (Listed Buildings and Conservation Areas) Act 1990 (amended) requires planning authorities to preserve or enhance the significance of heritage assets through the planning process. The NPPF places strong emphasis on the desirability of sustaining and enhancing the significance of heritage assets, and affords great weight to the asset's conservation. The NPPF defines a "heritage asset" as: "A building, monument, site place, area or landscape identified as having a degree of significance meriting consideration in planning decisions, because of its heritage interest".
- 10.37 Islington Policy CS9 includes views in its definition of Islington's heritage assets and historic environment, and Policies DM2.1, DM2.3 and DM2.4 require developments to successfully address local views, skylines and silhouettes (including strategic views), resisting the loss of views and vistas which contribute to the significance of a conservation area.
- 10.38 The site is not within a Conservation Area but adjoins the boundary of the Kings Cross Conservation Area and is visible from the New River Conservation Area. Therefore the development would affect the setting of a Conservation Area and in accordance with the Planning (Listed Buildings and Conservation Areas) Act 1990 and the NPPF there is a duty to preserve and/or enhance the character of the Conservation Areas.
- 10.39 London Plan Policies 7.11 and 7.12 set out a view management framework relating to the protected vista of St Paul's Cathedral from Kenwood viewing gazebo. The application site is on the boundary of the viewing corridor which passes the buildings to the west of the site. The building's overall height would be 48.4m above ordnance datum (AOD), which is less than the 52.1m AOD threshold at St Paul's Cathedral, and less than the approximate 70.2m AOD threshold plane (the height of the viewing plane where it crosses the site) as set out in the London View Management Framework. This would therefore be below the viewing plane threshold and would not affect the strategic view (and in any case, it is noted that within the protected view the building is hidden behind the towers at 200 Pentonville Road which completely hide the site from view). There would be no impact on this or any other London Plan protected views.
- 10.40 The site is also adjacent to local viewing corridor LV8 which runs along Pentonville Road. Policy DM2.4 (Protected Views) states that development should not infringe on any part of the defined view, and Appendix 1 to the DM Policies 2013 states that development

which further obstructs the view to St Pancras Station and Hotel (Chambers) buildings, from the viewing point (north pavement of Pentonville Road at the junction with Penton Street) will not be permitted. The part of St Pancras Station which is visible from the viewing point is the Grade I listed clock tower.

- 10.41 A townscape assessment was submitted with the application, including Accurate Visual Representations (AVRs) which demonstrate the visual impact of the proposal on views of the building from several points within the surrounding area. The townscape assessment also considered the impacts on views from the Kings Cross Conservation Area (adjacent to the site) and the New River Conservation Area (not adjacent but contiguous with the Kings Cross Conservation Area); and the impact on view LV8.
- 10.42 The proposal would not obscure views of the clock tower; therefore, it would technically comply with the description of the view set out in the DM Policies document, and Policy DM2.4. It is noted however that the St Pancras Clock tower is a Grade I listed structure, and that protection of the setting of a heritage asset can include views of the heritage asset. View 3A (West along Pentonville Road from junction with Rodney Street) is the same as locally protected view LV8, and demonstrates the impact that the proposal would have on longer views of the Clock Tower.



Fig 10.1 AVR View 3A West along Pentonville Road from the junction with Rodney Street.

- 10.43 From this position, the appreciation of the Clock Tower is substantially limited to its silhouette within the varied skyline. In this view, the east elevation of the roof extension would be highly visible, however it would be no higher than the clock tower, and it would not compete visually or detract from the wider setting. The impact of the proposal on this view would not be harmful to the silhouette of the clock tower or the skyline in which it sits. Historic England have raised no objection to the proposal, and informally stated that views of the Clock Tower from the west of the site are more prominent. The proposal would result in limited additional visual enclosure of views to the Clock Tower, and is considered to have a neutral impact on the locally protected view LV8, and wider views (and therefore setting) of the Grade I listed Clock Tower.
- 10.44 The townscape assessment highlighted that the proposal would be prominent from view 5. View 5 is of the building from the junction of Lorenzo Street and Kings Cross Road, and is the backdrop to both Kings Cross and New River conservation areas.



Fig 10.2 AVR View 5 north from the junction of Lorenzo Street and Kings Cross Road.

- 10.45 The view is prominent because the foreground building (the Ecuadorian Consulate building at 144-146 Kings Cross Road) is (at 2 storeys) lower than the prevailing 4-5 storey character; because the Chapter Kings Cross student accommodation tower at 200 Pentonville Road already dominates the skyline, and because the highly visible rear elevation of York House presents an existing 7 storey blank brick elevation which has a dominating appearance. The proposed roof extension would increase the silhouette of the backdrop to the conservation area in this position, and officers consider that some articulation is required to the retained blank rear elevation in order to offset this additional bulk. The height of the roof extension has been reduced through the course of the planning application by 1.5m (half a storey) to 1.5 storeys, and it would be clad in a zig-zag perforated metal cladding which would be partially translucent with an uncluttered but contrasting appearance. The applicant has omitted the proposed artwork from the original proposal and stated an intention to articulate the existing blank rear elevation with tinted brickwork or applied panels (to be secured by condition 3); this would go some way to offset additional bulk of the extension by alleviating the bulky appearance of the existing blank elevation and is considered a more appropriate architectural solution by the Council's design officer. The high quality and well considered design of the roof extension in combination with additional enlivening of the existing rear elevation would balance against the (reduced) additional height proposed, and would result in a neutral overall impact on the setting of the conservation areas.
- 10.46 The other views considered within the Townscape Assessment were agreed by officers for consideration, have been reviewed, and raise no issues in terms of the impact of the proposal on the wider townscape.
- 10.47 The proposal would not result in any harm to the character or setting of any other nearby heritage assets.

Detailed Design and Materials

- 10.48 The submitted design and access statement highlights the positive features of the building (good quality brick elevations, interesting detailing and partial rusticated concrete cladding), and also its shortcomings ("unfinished" elevations, small windows, poor frontage and street relationship). The proposal seeks both to alter the retained existing building, and to add three extensions to increase the amount and quality of office

provision. The design of the extensions draws on evolution of the existing design language rather than slavish continuation of the existing design, or complete contrast.

10.49 The proposal would increase the height of the building by 5.0 m (one additional storey with an enclosed plant area above), and would raise the parapet line of the building by 1.35m. The changes to the parapet line are modest compared with the scale of the building and are proposed in response to the architect's opinion that currently the building appears "unfinished" with no significant capping or framing to the top floor windows. The parapet would be constructed with the same brick as the existing building, with a dogtooth bond to reflect the existing detailing and avoid an awkward junction between new and old brick. At present the vertical bay windows extend to parapet height with no coping or parapet, and this additional parapet is considered to improve the proportions of the existing elevations. The roof extension would be clad in a perforated metal rain screen, with a zig-zag plan to reflect the detail of the ground floor brick extension. This would have a simple form, but with contextual and interesting detailing, and would be a well-considered addition. As the upper part would be a plant screen and would have no walls behind, the perforated cladding would result in a translucent appearance, resulting in this extension being read as a lightweight addition in contrast to the retained heavy brick elevations.

10.50 The ground floor and entrance of the building currently have very poor interaction with the pedestrian environment. The proposed 5 storey front extension would bring the building line forward to match the adjacent building line and heights. This would not be full width, and would moderate between the character of the 4-5 storey pavement-edge buildings to the west, and the larger buildings set back from the pavement to the east.



Fig 10.3 front extension (looking east towards Angel). Street trees to be retained, but omitted from image for clarity.



Fig 10.4: Full scale model of perforated brick cladding (constructed on site).



Fig 10.5: Double height entrance on West Elevation.

- 10.51 Due to the retained and adjacent forecourt areas, the side elevations of the extension would be unusually prominent. The proposal would introduce two new entrances on the side elevations of the front extension which would be visible when approaching the building from Kings Cross to the west, or Angel to the east. There would be double height glazed openings (to a double height entrance lobby) with offset fenestration on the upper floors. This would result in the front elevation reading as a secondary elevation rather than the principal elevation. In response to officer and DRP comments, there would also be ground level fenestration on the front elevation, set behind a strip of soft landscaping to add some human scale activity to the front elevation. The relationship between these single storey windows and the double height side elevation entrances would result in clear visual hierarchy and legible entrances. The front extension would be constructed as a timber framed glazed structure, with a perforated brick skin formed by 450mm long bricks to introduce glimpses through; a zig-zag plan.
- 10.52 Officers have reviewed a 1:1 scale model of this elevation on-site and are satisfied that it would have a high quality and interesting appearance, without appearing monolithic or oppressive. The new windows and doors would be punched into this deep façade, with metal surrounds and very deep reveals. These would be well considered and would leave enough of the brick cladding visible to allow its appreciation. The proposed front elevation would result in a distinctive appearance, which would draw on the design language of the existing building and would be a high quality and distinctive addition to its setting. The existing concrete paving would be removed and re-landscaped to provide an improved setting (see “Biodiversity, Landscaping and Trees” section below). The new landscaping would be designed to soften the visual impact of the building and improve its relationship with the pedestrian environment.
- 10.53 The proposed rear extension would be over 1-2 storeys (lower ground and upper ground) within the existing car parking area to the rear. This would include the infilling of the existing undercroft area beneath the rear wing of the building. The extension would be

mostly glazed with metal cladding and fascia detail and would have roof terraces over the lower ground and upper ground floor levels with glazed balustrading. The rear extension would be modest in scale, and would have a simple understated appearance which would be visually subordinate and appropriate to its position at the rear of the building (and adjacent to the service areas and blank side/rear elevations of the surrounding buildings).

10.54 In addition to the extensions, the proposal would introduce new fenestration to replace all of the existing windows and doors. The existing windows are vertically separated by solid spandrel panels; these would be replaced with glazing to introduce more light into the building. There would be a new light well on the front elevation which would be modest in scale and set back behind the landscaped forecourt. The ground floor front elevation would be opened up with large glazed openings inserted, and hit-and-miss brickwork inserted to the adjacent piers to add interest and detail.

10.55 No details of construction detailing, balustrading, windows etc. or material samples, were submitted with the application. The detailed design and materials as shown on the submitted plans are considered acceptable subject to a condition (no.3) requiring the Council's approval of the detailed design and materials to be used, in order to ensure a high quality appearance.

Design Summary

The proposed development is considered to be well designed, and would preserve the character and appearance of the adjacent conservation areas and would avoid harm to the protected view LV8 and the wider views of the Grade I listed St Pancras Clock Tower along Pentonville Road. The responses received from the Design Review Panel and the Council's design officer were generally positive and limited to detailed design matters which are to be secured by condition 3.

10.56 The proposed alterations and extensions would be appropriately scaled and would sensitively upgrade the existing elevations whilst introducing well thought through and detailed extensions. The proposal is considered to be acceptable in terms of its design and heritage impacts.

Accessibility

10.57 London Plan Policy 7.2 states that development should achieve the highest standards of accessible and inclusive design, by ensuring that developments: (i) can be used safely, easily and with dignity by all members of society; (ii) are welcoming and convenient with no disabling barriers, (iii) are flexible and responsive to peoples' needs and (iv) are realistic, offering more than one solution to future users.

10.58 Islington Policy DM2.2 requires all new developments to demonstrate inclusive design, including that all developments should demonstrate that they provide for ease of and versatility in use, deliver safe, legible and logical environments and produce places and spaces that are convenient and enjoyable to use for everyone. All development needs to be assessed against this policy background to ensure genuinely inclusive design from the outset and for the lifetime of the development.

10.59 The proposal would essentially remove all of the inherent accessibility barriers with the existing building, and by re-planning the entrances, WCs, circulation, and internal spaces would substantially upgrade the accessibility of the building. This would allow step free access to all of the building, level entrances to both Pentonville Road and Lorenzo Street

elevations (the rear entrance is closer to on-street parking for blue badge holders), and would avoid segregation of entrances or circulation spaces for users of varying mobility.

- 10.60 The proposal would result in the requirement for one additional blue badge parking space, and a financial contribution of £2,000 is to be secured through the s.106 agreement to enable the location of this (location shown indicatively as adjacent to the Lorenzo Street entrance).
- 10.61 The council's inclusive design officer provided feedback, and the applicants have responded by amending the plans to include 4 accessible cycle parking spaces, 2 mobility scooter charging points (close to the lift, with adequate turning circles and fire protection), circulation spaces with increased width, wheelchair refuge areas on each floor, and additional accessible WCs on each floor.
- 10.62 The wheelchair access to the building's rear entrance would follow the perimeter of the servicing area, and is shown on the plans as a marked out area. The potential for clashes is minor, but condition 13 (landscaping) includes the requirement for details as to how this area will be treated to clearly demarcate pedestrian priority.
- 10.63 The inclusive design and access measures proposed would be secured by an appropriately worded condition (no.4).
- 10.64 Following the amendments to the scheme and subject to the above conditions and planning obligation to facilitate on-street wheelchair parking, officers consider that the proposal would result in a good standard of inclusive design.

Neighbour Amenity

- 10.65 All new developments are subject to an assessment of their impact on neighbouring amenity; including in terms of daylight, sunlight, privacy, increased sense of enclosure, noise and disturbance as required by London Plan Policies 7.14 and 7.15 and Development Management Policy DM2.1.
- 10.66 It is noted that there are very few residential properties within the immediate vicinity, and that the potentially affected properties are limited to those on the other side of Pentonville Road (178-186 Pentonville Road), the two houses adjacent to the site (3A and 4 Lorenzo Street), and 160 and 160A Kings Cross Road to the south.
- 10.67 The majority of surrounding uses are either office/studio based businesses, student accommodation (Chapter Living at 200 Pentonville Road and Dinwiddy House at 189-205 Pentonville Road), or hotel (166-170 King's Cross Road).
- 10.68 Access to sunlight and daylight is considered beneficial for business uses although it is normal for this to be supplemented by artificial lighting so there are no particular standards for retained sunlight and daylight levels to these uses. Student accommodation is generally considered temporary accommodation (and many students will have additional accommodation out of term-time) and is linked to education institutions, which carry additional amenities (for example on-campus study spaces, communal social facilities, etc.) The residential amenity standards required for student accommodation are not protected in the same way as permanent conventional housing. Similarly, hotels are temporary accommodation and do not have the same requirements for good levels of sunlight and daylight as housing. Hotels and student accommodation are referred to in the sunlight and daylight report as "quasi residential" properties.

Daylight to existing buildings

- 10.69 The proposed increase in height would result in the potential for loss of daylight to neighbouring properties. To demonstrate the impacts, a sunlight and daylight assessment was submitted with the application. This considers the impacts of the proposed development on the residential neighbours in accordance with the 2011 Building Research Establishment (BRE) guidelines. It is noted that this assessment was carried out before the development was reduced in height, but as the impacts were demonstrated to be acceptable, it follows that the impacts of a reduced development are also considered acceptable.
- 10.70 With regard to daylight, the BRE guidance states that if the new development subtends an angle of more than 25° to the horizontal (taken perpendicular to the centre of the affected window), of a window to an existing building, then the diffuse daylighting of the rooms in that building may be adversely affected. The impact on neighbouring windows would be noticeable if either:
- the VSC [vertical sky component] measured at the centre of an existing main window is less than 27%, and less than 0.8 times its former value; (the VSC test);
 - the area of the working plane in a room which can receive direct skylight is reduced to less than 0.8 times its former value (the daylight distribution test).
- 10.71 The daylight tests were applied to the above mentioned residential properties near to the site. In total 85 windows and 59 rooms were modelled and analysed. All of the windows at the residential properties would comply with both BRE tests for daylight, thus there would be no noticeable impact on the daylight experienced by the neighbouring residential properties.
- 10.72 The daylight tests were also applied to the windows and rooms of the above mentioned hotel and student accommodation units near to the site. In total 494 windows and 331 rooms were modelled and analysed. All of the windows tested would comply with the VSC tests, and 311 out of the 331 rooms tested would pass the daylight distribution test. According to the BRE guidance, there would therefore be a “noticeable” reduction in daylight to 20 student rooms. The area of the working plane which could receive direct skylight in these 20 rooms would be between 73.7 and 79.8 times their former values, against a target of 0.8 times the former value. Whilst the reduction in daylight would therefore be “noticeable”, it would be minor and well within normally acceptable limits. Although these hotel and student rooms are not necessarily offered the same levels of protection as conventional residential units, the impacts on the daylight levels they would receive would nonetheless be negligible or minor.

Sunlight to existing buildings

- 10.73 With regard to sunlight, the BRE Guidelines state that neighbouring habitable rooms should be assessed if there is the potential for loss of sunlight, and main living rooms are considered more sensitive than kitchens and bedrooms. Loss of sunlight may be noticeable where:
- The new development is situated within 90° of due south of the (existing) window; and
 - The new development is within 25° to the horizontal (taken perpendicular to the window), measured from the centre of the window.
- 10.74 For windows which are likely to be affected, there would be no real noticeable loss of sunlight where in a typical year the centre point of the assessed window receives:

- More than 1 quarter (25%) of annual probable sunlight hours (APSH) including at least 5% of Annual Winter Probable Sunlight Hours (WSPH) between 21 Sept and 21 March (i.e. winter); or
- less than 0.8 of its former hours during either period

10.75 In cases where these requirements are breached there will still be no real noticeable loss of sunlight where the reduction in sunlight received over the whole year is no greater than 4% of APSH.

10.76 The sunlight tests were applied to the above mentioned residential properties and quasi-residential properties near to the site, and all of the windows would comply with both BRE tests for sunlight. The retained absolute levels of annual APSH will be between 20% and 95% while the retained absolute levels of WSPH will be between 6% and 29%

10.77 For the 20 student bedrooms which would experience noticeable (but acceptable) losses in daylight distribution, the retained levels of APSH and WSPH would be between 34% and 74% APSH and 10% to 23% WSPH respectively, which are well in excess of the BRE guidance targets.

Sunlight and daylight summary

10.78 All the residential properties around the site will be fully BRE compliant for both daylight and sunlight, and all of the hotel and student bedrooms will be fully BRE compliant for both daylight and sunlight, apart from 20 student bedrooms which will receive minor losses of daylight, but will retain excellent amounts of sunlight. The proposal would have no unacceptable impact on sunlight and daylight to surrounding properties.

Privacy, enclosure and overlooking

10.79 Policy DM2.1 identifies a minimum distance of 18 metres between windows 'to protect privacy for residential developments and existing residential properties. Standard 28 of the London Plan SPG Housing (2016) requires proposals to demonstrate that habitable rooms would have adequate levels of privacy in relation to neighbouring properties.

10.80 The nearest residential property is 4 Lorenzo Street, adjacent to the rear servicing area. The proposal would not result in any new windows within 18m of any surrounding residential buildings, and the new windows and outdoor terraces would either overlook Pentonville Road (to the front), the secondary elevations of the surrounding businesses (at ground floor level) or would be substantially higher than the surrounding buildings (in the case of the 8th floor roof extension and terrace). Additionally, the proposal would not include any residential accommodation, so any overlooking would only occur when the offices are in use and there would be no habitable rooms overlooking habitable rooms. There would be no unacceptable impact on privacy, enclosure or overlooking to nearby residents.

Noise and Disturbance

10.81 Whilst the proposed terraces would not result in unacceptable overlooking impacts they do have the potential to result in additional noise. There would be internal café areas which would be similar to staff kitchen areas, but the proposal does not include the sale of alcohol, or any bar/restaurant areas, and the noise impacts arising from workers working, smoking, or having lunch outside are unlikely to be significant. No concerns were raised by the Council's environmental health officer. However, in order to avoid unacceptable harm to neighbours condition 24 is recommended to limit use of the

outdoor areas until 7pm every day, with no music or licensable activities permitted outdoors.

- 10.82 The external plant would be split across a lower ground floor plant room, and a plant area at roof level, with the exception of a kitchen extract vent at first floor, venting into the servicing area. No noise assessment was provided, but given the high level location of extracts and vents and the distance to residential neighbours, no objection is raised to this subject to the imposition of a planning condition setting external noise limits at the nearest sensitive receptors (residential windows) and a condition (no.17) requiring details of any flues and extraction systems.
- 10.83 Officers consider that subject to the recommended conditions the proposal will not result in unacceptable impacts in terms of noise and activity.

Neighbour amenity summary

- 10.84 Subject to the conditions set out in this report, it is considered that the proposed development would not give rise to unacceptable impacts on neighbouring residential amenity. The proposal is thus considered acceptable in accordance with London Plan Policies 7.6, 7.14 and 7.15, and Development Management Policy DM2.1.

Biodiversity, Landscaping and Trees

- 10.85 London Plan Policy 2.18 states that development proposals should incorporate appropriate elements of green infrastructure that are integrated into the wider network, and Islington Policy DM6.5 states that Developments must protect, contribute to and enhance the landscape, biodiversity value, and growing conditions of the development site and surrounding area.
- 10.86 There are two street trees on the pavement outside the site, on Pentonville Road. These are managed by TfL who responded to the proposal raising no objection subject to the tree protection measures which are to be secured by condition 14.
- 10.87 The site is not within a conservation area, and there are 6 trees on the forecourt which are not offered formal protection. 4 of the 6 trees on the site are identified in the Arboricultural Report as category B trees (trees of moderate quality), and the other 2 are category U, which are unsuitable for retention. The proposal would remove all of the 6 trees on the site, and the submitted landscaping plan shows that these would be replaced with a tulip tree, 2 field maple trees, and soft planting as part of the landscaping works following the extensions. No objection has been raised to the loss of these trees and the proposed replacements, however the Council's tree officer noted that the replacement trees proposed are relatively small and are unlikely to provide sufficient canopy cover to adequately mitigate the loss of these trees. A condition (no.6) is therefore recommended requiring the submission of revised tree planting details, including sufficient tree pits (in terms of design, size and planting medium) in order to secure a suitable number of trees which will achieve suitable canopy cover in maturity.
- 10.88 A landscaping plan has been provided, with limited detail. This shows an appropriate outline design incorporating the proposed trees, but does not include detail on planting mixes, gradients, drainage, or materials. Subject to a condition (no.13) requiring further approval of details with regard to the landscaping, the area of landscaping would be acceptable.
- 10.89 The submitted ecology report identifies that there are unlikely to be bats living within the building due to its design, but that there are several protected and notable bird species

which could use the building and rooftops to nest, including species such as the Starling, House Sparrow and Black Redstart. The installation of bird boxes is therefore recommended, to be secured by condition 7.

- 10.90 Policy DM6.5 states that developments should maximise the provision of green roofs and the greening of vertical surfaces as far as reasonably possible, and where this can be achieved in a sustainable manner, without excessive water demand. Developments should use all available roof space for green roofs, subject to other planning considerations. All roofs should be biodiversity based extensive substrate roofs with a minimum substrate depth of 80-150mm. The sustainability statement states that a biodiverse roof can be provided over the front extension, and officers note that the other roofs would be mostly covered with a large (low profile) plant area. Condition 15 is recommended to ensure the provision of a biodiverse roof over the flat roof areas of the building unless adequately demonstrated to be unfeasible.

Security and External Lighting

- 10.91 Policy DM2.1 requires developments to be designed to be safe and to demonstrate safety in design; including access, materials and site management. Policy DM2.2 requires developments to deliver safe, legible and logical environments.
- 10.92 Paragraph 125 of the NPPF requires developments to limit the impact of light pollution from artificial light on local amenity, dark landscapes and nature conservation. Paragraph 7.19 (Policy 7.5) of the London Plan (MALP) 2016 states that the lighting of the public realm also needs careful consideration to ensure places and spaces are appropriately lit, and there is an appropriate balance between issues of safety and security, and reducing light pollution. Poorly designed lighting has the potential to add to the existing Light Pollution levels in London, to cause harm to neighbour amenity, and to disturb dark corridors for wildlife.
- 10.93 No details of external lighting were submitted with the application, although given the proposed permeable elevation design and additional ground floor glazing, additional external lighting is not considered necessary. A condition (no.8) is recommended requiring details of any external lighting to be approved by the Council, to avoid excessive light pollution and ensure a well-designed and safe environment in accordance with the above policies.

Health and Air quality

- 10.94 Policy 7.14 of the London Plan states that development proposals should minimise increased exposure to existing poor air quality and make provision to address local problems of air quality (particularly within Air Quality Management Areas (AQMA)). Policy DM6.1 requires developments to provide healthy environments, reduce environmental stresses, facilitate physical activity and promote mental well-being, and states that developments in locations of poor air quality should be designed to mitigate the impact of poor air quality to within acceptable limits.
- 10.95 Islington is an Air Quality Management Area in recognition of borough-wide poor air quality. An air quality assessment was submitted, including an Air Quality Neutral Assessment which concludes that as the pollutant emissions (nitrogen oxides) from the proposed plant would be lower than the GLA's Sustainable Design and Construction SPG benchmarks, no further mitigation is required regarding the ongoing emissions arising from the building's use.

- 10.96 Of additional concern cumulatively in London is the impact of the number of concurrent construction projects underway and the resultant harm to air quality. The proposal is relatively minor as it will not result in major demolition or excavation works, although there will still be construction dust, waste, machinery, material storage and vehicles which all have the potential to negatively impact air quality. The London Plan “Control of Dust and Emissions during Construction and Demolition” SPG requires low emission non-road mobile machinery (NRMM) to comply with low emissions standards and a condition (no.16) is recommended to ensure that the proposal complies with these standards.
- 10.97 The submitted air quality assessment sets out dispersion modelling used to analyse of the existing air quality, and the likely exposure of workers to elevated pollutant levels. This considers the air quality at ground level as a “worst case scenario” (as particle levels are lower at elevated heights) and concludes that as the proposed use (office use) is not as sensitive as other uses, the site is considered suitable for this use without mitigation measures.
- 10.98 Subject to compliance with condition 16, the proposal is considered acceptable in terms of Air Quality.

Highways and Transportation

- 10.99 The site is on the junction of Pentonville Road and Lorenzo Street. The main (pedestrian) entrance fronts Pentonville Road, and there is vehicular access to the rear of the site via a gate on Lorenzo Street.
- 10.100 Pentonville Road is part of the Transport for London (TfL) Road network. The single red lines on Pentonville Road prevent stopping between 07:00 and 19:00 Monday to Saturday. A loading bay on Pentonville Road is located approximately 40m from the Site’s entrance which prevent stopping between 07:00 and 19:00 Monday to Saturday, except for 20 minutes loading between 10:00 and 16:00.
- 10.101 Islington Council is the highway authority for Lorenzo Street. Lorenzo Street has double yellow lines along its western side and a mixture of single yellow line and resident permit bays on the eastern side. The site is within a Controlled Parking Zone (CPZ) which operates between 08:30 and 18:30 Monday to Friday, and 08:30 and 13:30 on Saturdays. Loading or unloading is permitted on yellow lines for up to 40 minutes.
- 10.102 The site has a PTAL (Public Transport Accessibility Level) of 6B (the highest possible level) which is indicative of excellent accessibility by public transport. The site is served by 15 bus routes within 400m; and is within 725m of St Pancras and Kings Cross rail and underground stations with National Rail, Eurostar and underground services (Circle, Hammersmith & City, Metropolitan, Northern, Piccadilly and Victoria lines).

Sustainable Transport

- 10.103 A full travel plan is required for developments equal to or more than 2,500sqm. Although the uplift in floorspace would be less than this, and the building would be used by multiple occupiers (all likely to have a significantly lower area than this), the building would be managed by one workspace provider, and facilities such as collection and deliveries, and cycle storage, lockers and showers, would be managed by the overarching provider. It is therefore appropriate for one travel plan to be produced for the building, rather than multiple transport statements produced by the individual businesses using the building.
- 10.104 The proposal is accompanied by a Transport Assessment and a Framework Travel Plan, which identify very low likely levels of car usage (3%) and heavy reliance on public

transport. This aims to reduce reliance on public transport by promoting cycling and walking, and sets out appropriate aims, measures, and monitoring approaches.

- 10.105 A Pedestrian Environment Review System (PERS) audit was requested by officers and submitted with the application to identify obstacles to pedestrian movements. This has been reviewed by officers and highlights three potential obstacles to wheelchair accessibility to pedestrian movement: the gradient of Lorenzo Street; the highly trafficked/unattractive pedestrian environment; and minor footway maintenance issues (cracked pavements and uneven surfaces) on Lorenzo Street. The first two of these are symptomatic of the site's location and topography, although it is noted that there would be accessible entrances to the building on both street elevations and that the proposed landscaping would go some way to improving the immediate pedestrian environment. The third issue (highway maintenance) relates to routine maintenance rather than poor design and no physical obstacles have been identified (kerbs, junctions, bollards, railings etc.) for improvement to enable better pedestrian links. All 6 pedestrian links and 10 crossings within the PERS audit were given a "green" light (within a traffic light scoring system).
- 10.106 The results of the Transport Assessment have been considered, and the aims of the framework travel plan are supported by officers, with a full travel plan to be secured by the s.106 agreement, as set out in the Planning Obligations SPD.

Servicing and refuse

- 10.107 Policy DM8.6 (Delivery and servicing for new developments), Part A states that for commercial developments over 200 square metres, delivery/servicing vehicles should be accommodated on-site, with adequate space to enable vehicles to enter and exit the site in forward gear (demonstrated by a swept path analysis). Where servicing/delivery vehicles are proposed on street, Policy DM8.6 (Delivery and servicing for new developments), Part B, requires details to be submitted to demonstrate that on-site provision is not practical, and show that the on-street arrangements will be safe and will not cause a traffic obstruction/nuisance.
- 10.108 A draft Delivery and Servicing & Waste Management and Collection Plan was submitted with the application, outlining the proposed servicing arrangements.
- 10.109 The application site is currently served by a servicing and car parking area (to the south of the building), accessed from Lorenzo Street (to the east). The car parking area would be removed, but the servicing area would be retained with sufficient space for a transit size vehicle to enter the site, turn around, and leave in a forward gear. The majority of servicing would therefore take place on site.
- 10.110 The double yellow lines on Lorenzo Street allow stopping for 40 minutes for loading, but this should be avoided in favour of a managed solution where possible. There is a loading bay on Pentonville Road approximately 40m from the Site's entrance which will be used by larger vehicles. This loading bay allows no stopping at peak hours (between 07:00 and 10:00, and between 16:00 and 19:00 Monday to Saturday), and 20 minutes loading between these times. TFL have responded to the application and raise no objection to the use of this loading bay, given that the majority of servicing would take place on site.
- 10.111 For refuse and recycling storage, the Council's waste guidance note suggests 2,600L of waste storage per 1,000sqm of floorspace for B1a uses, based on collections occurring once a week. For the proposed floorspace, the number of Eurobins (1,100 litres capacity)

required would be 16 Eurobins. The draft servicing plan sets out the standard arrangements used by the Office Group, which co-ordinates collections across the group's sites and arranges collection approximately 3 times a week. The proposal includes provision for 2 Eurobins for recycling and 2 for general waste (4,400L in total). This is approximately 10% less than the Council's guidance, but given that it would be a managed arrangement and is based on the servicing requirements across other similar sites (which provide a more up to date barometer than the Council's general guidance for all office users), this is considered sufficient.

10.112 Collections and deliveries would be managed via the on-site servicing yard and the loading bay on Pentonville Road, and would be arranged to take place outside peak hours. Vehicle Routing and tracking diagrams are set out within the draft Delivery and Servicing & Waste Management and Collection Plan, and no objection is raised from the Council's Highways Officer.

10.113 The draft Delivery and Servicing & Waste Management and Collection Plan is a draft document intended to outline principles for discussion and agreement with officers. However, this is a comprehensive document and is considered sufficient to manage the impacts of the development. Condition 22 is recommended securing compliance with the submitted plan unless variation is sought, in which case further approval by the LPA will be required.

Vehicle and Cycle parking

10.114 The proposed development would have 70 cycle spaces for employees (long stay) at lower ground floor level, and 4 parking spaces for recumbent/cargo style bikes for disabled cyclists and 7 standard spaces for visitors within a covered enclosure in the service yard. These would have step free access and would be secured, attractive and sheltered. There would also be showers, lockers and changing facilities. Eight additional cycle parking spaces are proposed within the servicing area for visitor use. The proposed cycle storage would comply with the Islington Cycle Parking standards (Appendix 6 of the DM Policies) and would be acceptable. Condition 21 is recommended to secure provision of the proposed cycle storage and facilities.

10.115 No vehicle parking is proposed on-site. A financial contribution of £2,000 is proposed to secure an on-street blue badge parking bay. A location on Lorenzo Street is identified by the Transport Assessment and other documents; this demonstrates that it would be possible to deliver the parking space although the final location will be determined by the Council's Highways Officers. The financial contribution is to be secured by the s.106 agreement.

Construction impacts

10.116 A draft Construction Traffic Management Plan (CTMP) was submitted, outlining measures for the routing, accommodation, loading and unloading of construction vehicles. Construction vehicles will make use of the service yard, and partial suspension of car parking spaces and a temporary loading area on Lorenzo Street, to avoid obstruction of Pentonville Road. The plan does not outline a construction programme, but states that an indicative construction programme will be provided within the final CTMP and once a contractor has been appointed. This will set out indicative timescales for each phase of construction.

- 10.117 Draft measures are outlined for minimisation of environmental and amenity impacts, and working hours of 08.00-18.00 Monday to Friday, 08.00-13.00 on Saturdays, and none on Sundays or Bank Holidays. These hours are in line with those permitted by the Islington Noise Service code of practice for construction sites.
- 10.118 As the CTMP was drafted well in advance of construction works, this outlines headline impacts and intentions for minimisation of impacts, but does not include specific information on wider vehicle movements, dates of deliveries, or reference to the upcoming Kings Cross Gyratory works, which may affect the site depending on timescales. A condition (no.9) is therefore recommended to secure an expanded CTMP detailing specific measures, and expanded to take account of other nearby developments, highway works, and notification of neighbours.
- 10.119 Any requirement for the repair and re-instatement of the footways and highways adjoining the development which arises from construction impacts, should be resourced by the applicant, and secured by a s.106 obligation. This is committed to in the application documents and the cost is to be confirmed by LBI Highways, paid for by the applicant and the work carried out by LBI Highways.
- 10.120 Subject to compliance with an expanded construction management plan (and recommended condition 9), the proposal would make all reasonable efforts to avoid unacceptable impacts to neighbour amenity, the wider environment, or the safe and efficient operation of the highway network.
- 10.121 In the interest of protecting neighbouring residential amenity during the construction phase of the development (having regard to impacts such as noise and dust) the applicant is also required to comply with the Council's code of construction practice. Compliance would need to be secured as part of a section 106 agreement together with a payment of £1,005 towards monitoring. This payment is considered an acceptable level of contribution having regard to the scale of the development, the proximity of other properties, and likely duration of the construction project.

Highways and Transportation Summary

- 10.122 The application sets out adequate provision for on-site (and partial on-street) servicing, waste storage, blue badge parking, cycling, collections and deliveries, and includes a draft travel plan which sets out continued measures to promote sustainable modes of transport. The proposal would be acceptable and would comply with Islington Core Strategy (2011) Policies CS11 and CS13; Islington Development Management Policies DM5.1, DM8.2, DM8.5 and 8.6; and the London Plan SPG Land for Industry and Transport (September 2012).

Sustainability, Energy Efficiency and Renewable Energy

- 10.123 Islington Core Strategy Policy CS10 seeks to minimise Islington's contribution to climate change and ensure that the borough develops in a way which respects environmental limits and improves quality of life. This requires all development to achieve the highest feasible sustainability standard, and a sustainability statement was submitted which follows the structure suggested by the Mayor of London's Supplementary Planning Guidance (SPG) Sustainable Design and Construction, and London Plan Policy 5.3.

Flooding, Water Consumption, and Sustainable Urban Drainage Systems (SUDS)

- 10.124 Policy DM6.6 expects all major development to include details to demonstrate that SUDS has been incorporated and will be properly maintained.

- 10.125 The existing drainage within the vicinity of the site is to the combined sewer for both surface water and foul water. As the proposal relates to an existing building on a built-over site, there is limited scope for a new SUDS. However in order to maximise the potential to reduce pressure on the sewer network, the proposal would take opportunities to introduce facilities which would follow SUDS principles.
- 10.126 There would be a green roof over the front extension of the building (the other roofs will be used for terraces and plant) which will provide an element of rainwater storage and slow drainage into the sewers. The drainage system will be designed to discharge into the combined sewer, but with a demarcation chamber and separated foul and surface water systems which would allow separation of foul sewage in the event that a separate surface water sewer is delivered in future. There would also be a geo-cellular storage unit with a capacity of 6m³ and a vortex hydro brake to slow discharge of water into the sewer.
- 10.127 London Plan Policy 5.15 and Islington Policies CS10, DM6.6 and DM7.4 require developments to minimise water consumption and the pressure on the combined sewer network, by incorporating water efficiency measures. The submitted sustainable design and construction statement states that the potential for rainwater harvesting is being explored. Given that the proposal would not achieve a full SUDS, and that the majority of the roof would not be given over to biodiverse roofs, rainwater recycling should be provided unless it is demonstrated that it is not feasible to do so (as required by Policy CS10 and the Environmental Design SPD), so condition 25 is recommended to secure this.
- 10.128 Whilst the proposal would not incorporate full SUDS, it would make reasonable efforts to minimise pressure on the combined sewer. Condition 15 is recommended to ensure that the green roof is provided, and condition 10 is recommended to ensure that the measures set out in the Drainage Strategy are carried out. The net result is that following the development there would be less pressure on the existing public sewer network, and the proposal is considered acceptable in this respect.

Energy Efficiency, CO2 Emissions, and Renewable Energy

- 10.129 London Plan Policy 5.2B sets out a CO₂ reduction target, for regulated emissions only, of 40% against Building Regulations 2010 and 35% against Building Regulations 2013. Islington Policy CS10 A and Section 2 of the Environmental Design SPD require that onsite total CO₂ reduction targets (both regulated and unregulated) against Building Regulations 2010 are reduced by 40% where connection to a Decentralised Energy Network (DEN) is possible, and 30% where not possible. These targets have been adjusted for Building Regulations 2013 to reductions of 39% where connection to a DEN is possible, and 27% where not possible.
- 10.130 The proposal would take a fabric-first approach to CO₂ savings, and would incorporate upgrades to the existing building envelope, passive ventilation, solar PV panels, and an air source heat pump, to achieve a carbon reduction of 32.1% (regulated and unregulated, against a target of 30%) or 48.1% (regulated, against a target of 35%). This would also represent a 23.8% reduction compared to the existing building. This would comply with the above targets and would result in reduced carbon emissions overall.
- 10.131 Policy DM 7.4A states “Major non-residential developments are required to achieve Excellent under the relevant BREEAM or equivalent scheme and make reasonable endeavours to achieve Outstanding”. The council’s Environmental Design Guide states

“Schemes are required to demonstrate that they will achieve the required level of the CSH/BREEAM via a pre-assessment as part of any application and subsequently via certification.” The proposal would involve refurbishment and new-build elements, and the Sustainable Design and Construction Statement sets out the principles of the BREEAM assessment and how the proposal would comply with the relevant criteria to achieve an “Excellent” rating. A condition (no. 18) is recommended to secure this.

- 10.132 London Plan Policy 5.6B sets out a hierarchy for energy systems for major development proposals, prioritising connection to existing heating or cooling networks; over a site wide CHP network and communal heating and cooling. Islington Policy DM7.3B states “*all major developments within 500 metres of an existing or planned DEN.... are required to submit a feasibility assessment of connection to that network, to determine whether connection is reasonably possible.*” The proposal does not intend to provide a CHP and there are no DENs within 500m. However, an obligation is recommended within the s.106 agreement to safeguard future connection if it becomes available and is feasible in future.
- 10.133 In accordance with the Council’s zero carbon policy, the council’s Environmental Design SPD states that “after minimising CO2 emissions onsite, developments are required to offset all remaining CO2 emissions (Policy CS10) through a financial contribution”. The Environmental Design SPD states “The calculation of the amount of CO2 to be offset, and the resulting financial contribution, shall be specified in the submitted Energy Statement.”
- 10.134 The proposed works would minimise carbon emissions arising from the building, and in order to comply with the zero carbon policy it is proposed that the carbon offset contribution of £231,702 (see “Planning Obligations and CIL” below) is secured by the s.106 agreement.

Building Fabric

- 10.135 In accordance with Islington Policies CS10 and DM7.4, details on the materials selection based on lifecycle assessment for all major material components of the design should be provided e.g. structure, steel, cladding, concrete etc. These details were not supplied with the application, so a green procurement plan is recommended to be required by a planning condition (no.12).

Contamination

- 10.136 Paragraphs 120-122 of the NPPF state that to prevent unacceptable risks from pollution and land instability, planning policies and decisions should ensure that new development is appropriate for its location. The effects (including cumulative effects) of pollution on health, the natural environment or general amenity, and the potential sensitivity of the area or proposed development to adverse effects from pollution, should be taken into account. Planning decisions need to consider whether the site is suitable for its new use taking account of ground conditions and natural hazards or former activities such as pollution arising from previous uses; and in doing so, local planning authorities should focus on whether the development itself is an acceptable use of the land. London Plan Policy 5.21 states that appropriate measures should be taken to ensure that development on previously contaminated land does not activate or spread contamination. Proposals should include an assessment of existing ground conditions and identify appropriate remedial measures for any contaminated land prior to development commencing.

- 10.137 The existing building is located on or near to land which was previously used for a range of potentially contaminating uses, including railway construction; iron works, a boot warehouse, and WWII bombing. A number of potential sources of contamination and associated risks have been identified, and intrusive investigation is proposed, including soil sampling and monitoring wells, to determine whether the proposed excavation works (for the proposed light well, services and foundations) would result in exposure to contamination. In order to avoid exposing workers to contamination risks, condition 19 is recommended to require investigation, and remediation and monitoring if required.

Sustainability Summary

- 10.138 No overall objection is raised on sustainability grounds, and as set out above, it is recommended that the relevant sustainability requirements are secured by planning conditions and s.106 obligations.

Fire Safety

- 10.139 Part B of the London Plan policy 7.13 states that development proposals should contribute to the minimisation of potential physical risks, including those arising as a result of fire. An outline fire safety strategy was submitted as part of the application.
- 10.140 The London Fire and Emergency Planning Authority and the council's Building Control team were consulted on the proposed development. Neither consultee raised an objection to the proposed development. The Council's Building control officer stated that the proposal includes a dry riser (allowing access for fire hoses on each floor), separate entrances and staircases, fire rated refuge areas, and separate access to the firefighting staircase and lift for the fire brigade. The only consideration not explicitly addressed is the potential for external fire spread to other buildings, which can be addressed through the selection of materials and glazing through the building control assessment process. An informative (no.10) has been included in the recommendation to remind the applicant of this consideration.

Basement Development

- 10.141 The Islington Basement Development SPD was adopted in January 2016 and sets out requirements for the Council's application of planning policies in relation to basements. This includes the need for planning applications to be accompanied by Structural Method Statements (SMS) signed by a chartered Civil Engineer (MICE) or Chartered Structural Engineer (MIStruct.E).
- 10.142 The proposal includes partial excavation within the forecourt to provide a new lightwell and natural daylight at lower ground floor level, set within the new area of hard and soft landscaping.
- 10.143 A SMS was submitted with the application which describes the site investigation (carried out using three boreholes and two trial pits), and consideration of groundwater flow; impacts to neighbours (and the ground surrounding the London Underground tunnel), the stability of the (sloped) land, surface water flow, and impacts on trees. No objections were raised by London Underground (Infrastructure Protection) or Thames Water. The SMS was signed by engineers with the above qualifications, and explains in detail the design and construction process which are proposed in order to ensure that the lightwell construction has no significant adverse effect on the surface and subterranean water regimes, or on slope stability, with risks managed through all stages of construction.

10.144 The proposed excavation would be relatively minor, and the SMS demonstrates that this would be constructed without unacceptable impacts on the surrounding land. Subject to a condition (no.26) requiring the proposed lightwell to be constructed in accordance with the SMS (in addition to the provisions of the Construction and Environmental Management Plan secured by condition 9 and the Code of Construction Practice secured by the s.106 agreement) the proposed excavation would be acceptable.

Planning Obligations and CIL

10.145 If the application is approved and the development is implemented, a liability to pay the Islington Community Infrastructure Levy (CIL) and Mayor of London CIL will arise. CIL is intended to consolidate financial contributions towards the development's local infrastructure impacts, and additional separate contributions should not be sought towards the same infrastructure unless there is an exceptional and demonstrable need as a direct result of the proposed development.

10.146 Any further planning obligations which are not covered by the CIL payment should be sought through a legal agreement under s.106 of the Town and Country Planning Act, (1990, amended) and need to comply with the statutory tests set out in the NPPF and CIL Regulations 2010 (amended) to avoid unjustified double counting.

10.147 Islington's CIL Regulation 123 infrastructure list specifically excludes measures that are required in order to mitigate the direct impacts of a particular development and if specific off-site measures are required to make the development acceptable these should be secured through a s.106 agreement.

10.148 In order for the development to mitigate its own direct impacts, and to be acceptable in planning terms the following heads of terms are recommended, secured by a s.106 agreement.

- The repair and re-instatement of the footways and highways adjoining the development. Conditions surveys may be required. The cost is to be confirmed by LBI Highways, paid for by the applicant and the work carried out by LBI Highways.
- Compliance with the Code of Employment and Training
- Compliance with the Council's Code of Local Procurement
- 1 construction training placement (or if it can be demonstrated that this is not possible, a £5,000 contribution towards placements elsewhere).
- Local employment and training contribution of £13,464.
- Compliance with Islington's Code of Practice for Construction Sites and monitoring costs of £1,005.
- Accessible parking contribution of £2,000.
- Carbon Offsetting payment of £231,702 (index linked).
- Connection to a local energy network, if technically and economically viable. In the event that a local energy network is not available or connection to it is not economically viable, the developer should develop an on-site solution and/or connect to a neighbouring site (a Shared Heating Network) and future proof any on-site solution so that in all cases (whether or not an on-site solution has been provided), the development can be connected to a local energy network if a viable opportunity arises in the future.

- Submission of, and compliance with, a Green Performance Plan
- Crossrail funding contribution of £140,700 (to be offset against the Mayoral CIL payment).
- Affordable housing contribution of £160,800
- Approval and compliance with a draft full travel plan to be submitted to the Council for approval prior to occupation; a full travel plan to be submitted to the Council for approval within 6 months of first occupation (including a full travel survey); and a travel plan update to be submitted to the Council for approval three years after first occupation.
- The Council's legal fees in preparing the S106 and officer's fees for the monitoring and implementation of the S106 agreement.

11. SUMMARY AND CONCLUSION

- 11.1 As set out in the above assessment, the proposal has been assessed against the development plan and the comments made by residents and consultees.
- 11.2 Consequently, it is considered that the proposed development would comply with the relevant national, London Plan, and local planning policies (including the Islington Core Strategy, the Islington Development Management Policies and associated Supplementary Planning Documents).
- 11.3 It is recommended that planning permission is granted subject to conditions and s106 legal agreement heads of terms as set out in Appendix 1 - RECOMMENDATIONS.

APPENDIX 1 – RECOMMENDATIONS

RECOMMENDATION A

That planning permission be granted subject to the prior completion of a Deed of Planning Obligation made under section 106 of the Town and Country Planning Act 1990 between the Council and all persons with an interest in the land (including mortgagees) in order to secure the following planning obligations to the satisfaction of the Head of Law and Public Services and the Service Director, Planning and Development / Head of Service – Development Management or, in their absence, the Deputy Head of Service:

- The repair and re-instatement of the footways and highways adjoining the development. Conditions surveys may be required. The cost is to be confirmed by LBI Highways, paid for by the applicant and the work carried out by LBI Highways.
- Compliance with the Code of Employment and Training
- Compliance with the Council's Code of Local Procurement
- 1 construction training placement (or if it can be demonstrated that this is not possible, a £5,000 contribution towards placements elsewhere).
- Local employment and training contribution of £13,464.
- Compliance with Islington's Code of Practice for Construction Sites and monitoring costs of £1,005.
- Accessible parking contribution of £2,000.
- Carbon Offsetting payment of £231,702 (index linked).
- Connection to a local energy network, if technically and economically viable. In the event that a local energy network is not available or connection to it is not economically viable, the developer should develop an on-site solution and/or connect to a neighbouring site (a Shared Heating Network) and future proof any on-site solution so that in all cases (whether or not an on-site solution has been provided), the development can be connected to a local energy network if a viable opportunity arises in the future.
- Submission of, and compliance with, a Green Performance Plan
- Crossrail funding contribution of £140,700 (to be offset against the Mayoral CIL payment).
- Affordable housing contribution of £160,800
- Approval and compliance with a draft full travel plan to be submitted to the Council for approval prior to occupation; a full travel plan to be submitted to the Council for approval within 6 months of first occupation (including a full travel survey); and a travel plan update to be submitted to the Council for approval three years after first occupation.
- The Council's legal fees in preparing the S106 and officer's fees for the monitoring and implementation of the S106 agreement.

That, should the Section 106 Deed of Planning Obligation not be completed within 2 weeks from the date of the Planning committee meeting when a resolution to approve the application is reached (or a future date as agreed by officers and the applicant), the Service Director, Planning

and Development / Head of Service – Development Management or, in their absence, the Deputy Head of Service may refuse the application on the grounds that the proposed development, in the absence of a Deed of Planning Obligation is not acceptable in planning terms.

ALTERNATIVELY should this application be refused (including refusals on the direction of The Secretary of State or The Mayor) and appealed to the Secretary of State, the Service Director, Planning and Development / Head of Service – Development Management or, in their absence, the Deputy Head of Service be authorised to enter into a Deed of Planning Obligation under section 106 of the Town and Country Planning Act 1990 to secure the heads of terms as set out in this report to Committee.

RECOMMENDATION B

That the grant of planning permission be subject to **conditions** to secure the following:

List of Conditions:

1	Commencement (Compliance)
	<p>CONDITION: The development hereby permitted shall be begun not later than the expiration of three years from the date of this permission.</p> <p>REASON: To comply with the provisions of Section 91(1) (a) of the Town and Country Planning Act 1990 as amended by the Planning and Compulsory Purchase Act 2004 (Chapter 5).</p>
2	Approved plans list (Compliance)
	<p>CONDITION: The development hereby approved shall be carried out in accordance with the following approved documents and plans:</p> <p>Approved Documents: Cover Letter (Lichfields, 5 May 2017); Second cover letter describing amendments to the proposal (Lichfields, 23 June 2017); Design and Access Statement (DMFK, May 2017) to be read with "Addendum to DAS Statement" (DMFK); Planning and Consultation Statement (Lichfields, May 2017); Air Quality Assessment (REC, March 2017); Daylight & Sunlight Assessment 1089 (Point 2 Surveyors, March 2017); Draft Construction Traffic Management Plan (Caneparo Associates Ltd, May 2017); Draft Delivery and Servicing & Waste Management and Collection Plan (Caneparo Associates, May 2017); Drainage Strategy Report Ref: J3004-C-RP-0002 (Webb Yates Engineers Ltd, 30/03/17); Ecological Appraisal and Bat Building Inspection (Ecological Planning & Research Ltd, 21 February 2017); Fire Engineering Overview Report 620820-MLM-ZZ-XX-TN-F-0002 rev.02 (MLM Group, July 2017); Framework Employee Travel Plan (Caneparo Associates Ltd, May 2017); Heritage Impact Assessment (Lichfields, May 2017); Pedestrian Environment Review System (PERS) Audit (Caneparo Associates Ltd, April 2017); Phase 1 Environmental Risk Assessment (sic) 17.9411 (Constructive Evaluation, February 2017); Phase 2: Site Investigation Report 17.9411 Constructive Evaluation, February 2017); Structural Method Statement (Basement Impact Assessment) J3004-S-RP-0003 rev.01 (Webb Yates Engineers Ltd, 28/06/17); Sustainable Design and Construction Statement V3 (EB7, June 2017); Townscape and Visual Impact Assessment excluding Appendix 3 (Lichfields, May 2017); Townscape and Visual Impact Assessment Appendix 3 (Lichfields, June 2017); Townscape and Visual Impact Assessment Addendum (Lichfields, June 2017); Transport Assessment (Caneparo Associates Ltd, May 2017); Tree Information to Stage A & B AP/9352/WDC (Ian Keen Limited); Ventilation Strategy 3569/VS Rev A (Environmental Engineering Partnership, June 2017).</p> <p>Approved Plans Existing Site Location Plan 2065 A01; Existing Site Plan 2065 A05; Existing Lower Ground Floor Plan 2065 A09; Existing Upper Ground Floor Plan 2065 A10; Existing First Floor Plan 2065 A11; Existing Second Floor Plan 2065 A12; Existing Third Floor Plan 2065 A13; Existing Fourth Floor Plan 2065 A14; Existing Fifth Floor Plan 2065 A15; Existing Sixth</p>

Floor Plan 2065 A16; Existing Roof Floor Plan 2065 A17; Existing North Elevation 2065 A20; Existing East Elevation 2065 A21; Existing South Elevation 2065 A22; Existing West Elevation 2065 A23; Existing Section AA 2065 A30; Existing Section BB 2065 A31; Existing Section CC 2065 A32; Existing Section DD 2065 A33; Demolitions Lower Ground Floor A59; Demolitions Upper Ground Floor Plan 2065 A60; Demolitions First Floor Plan 2065 A61; Demolitions Second Floor Plan 2065 A62; Demolitions Third Floor Plan 2065 A63; Demolitions Fourth Floor Plan 2065 A64; Demolitions Fifth Floor Plan 2065 A65; Demolitions Sixth Floor Plan 2065 A66; Demolitions Roof Floor Plan 2065 A67; Demolitions North Elevation 2065 A70; Demolitions East Elevation 2065 A71; Demolitions South Elevation 2065 A72; Proposed Site Plan 2065 A50; Proposed Lower Ground Floor Plan 2065 A99 A; Proposed Upper Ground Floor Plan 2065 A100 A; Proposed First Floor Plan 2065 A101 A; Proposed Second Floor Plan 2065 A102 A; Proposed Third Floor Plan 2065 A103 A; Proposed Fourth Floor Plan 2065 A104 A; Proposed Fifth Floor Plan 2065 A105 A; Proposed Sixth Floor Plan 2065 A106 A; Proposed Seventh Floor Plan 2065 A107 A; Proposed Roof Plan 2065 A109 A; Proposed North Elevation 2065 A200 A; Proposed East Elevation 2065 A202 A; Proposed South Elevation 2065 A203 A; Proposed West Elevation 2065 A204 A; Proposed Roof Extension Details 2065 A2700 A; Proposed Section AA 2065 A300 A; Proposed Section BB 2065 A301 A; Proposed Section CC 2065 A302 A; Proposed Section DD 2065 A303 A; Proposed Landscaping 2065 A600; Proposed Landscaping Elevations 2065 A605; Proposed Landscaping Elevations 2065 A606; Proposed Landscaping Elevations 2065 A607; Proposed Landscaping Elevations 2065 A608; Proposed Lightwell Plan 2065 A2000; Proposed Lightwell Elevations 01, 02 and 03 2065 A2100; Proposed Lightwell Elevation 04 2065 A2200; Proposed Front Extension Detailed Elevations 2065 A2500.

REASON: To comply with Section 70(1) (a) of the Town and Country Planning Act 1990 as amended and also for the avoidance of doubt and in the interest of proper planning.

3 Materials and detailing (Prior to specific works)

CONDITION: Details and samples of all facing materials shall be submitted to and approved in writing by the Local Planning Authority prior to the commencement of superstructure works unless otherwise agreed in writing by the Local Planning Authority. The details and samples shall include large scale drawings, manufacturers details and material samples of the following:

- a) External facing materials, and in the case of new brickwork details bond, mortar colour and pointing style;
- b) windows and doors (including sections and reveals);
- c) details of any louvres, ventilation panels or screens;
- d) any external handrails or balustrading;
- e) copings, soffits, cills and reveals (and details of how these will be designed to avoid watermarks or staining to the surfaces below), the undersides of any projecting elements, and junctions of external materials including expansion gaps;
- f) Roof materials and edge details;
- g) Rainwater goods (including locations, fixings, material and colour);
- h) Details and location of all soil, vent and waste pipes which shall (except for the termination) be constructed within the building;
- i) Details of any other equipment or devices to be installed externally external surfaces of

	<p>the building including meter boxes, service connection access, aerials and satellite dishes;</p> <p>j) Details of the materials and detailing of the roof level plant enclosure;</p> <p>k) Details of articulation to the existing south elevation to offset the impact of the additional bulk resulting from the roof extension;</p> <p>l) Any external boundary treatments or gates;</p> <p>m) All other external materials.</p> <p>The development shall be carried out strictly in accordance with the details so approved and shall be maintained as such thereafter.</p> <p>REASON: In the interest of securing sustainable development and to ensure that the resulting appearance and construction of the development is of a high standard.</p>
4	Inclusive Design (Compliance)
	<p>CONDITION: Notwithstanding the approved plans the scheme shall be constructed in accordance with the principles of Inclusive Design, and all lifts and accessible WCs serving the accommodation hereby approved shall be installed and operational prior to the first occupation of the development hereby approved.</p> <p>The development shall be carried out strictly in accordance with the details so approved, shall be maintained as such thereafter.</p> <p>REASON: In order to facilitate and promote inclusive and sustainable communities, in accordance with policy 7.2 of the London Plan 2016, Policies CS7 and CS9 of the Islington Core Strategy 2011 and Islington's Development Management Policy DM2.2.</p>
5	Plant Noise and Fixed Plant (Prior to specific works)
	<p>CONDITION: Notwithstanding the approved plans and prior to the installation of any external plant, a detailed acoustic report shall be submitted to and approved in writing by the Local Planning Authority. The report shall be drafted by an appropriately experienced & competent person, to detail the specific individual and cumulative noise levels arising from the proposed plant, assess the noise impacts and set out relevant noise mitigation measures which shall be installed before commencement of the use hereby permitted and permanently retained thereafter.</p> <p>Notwithstanding the details approved, the design and installation of new items of fixed plant shall be such that when operating the cumulative noise level LAeq Tr arising from the proposed plant, measured or predicted at 1m from the facade of the nearest noise sensitive premises, shall be a rating level of at least 5dB(A) below the background noise level LAF90 Tbg. The measurement and/or prediction of the noise should be carried out in accordance with the methodology contained within BS 4142: 2014.</p> <p>REASON: To avoid unacceptable harm to neighbour amenity and to secure an appropriate residential environment for neighbouring occupiers.</p>
6	Trees (Prior to Specific Works)

	<p>CONDITION: Notwithstanding the hereby approved details, and prior to the removal of any of the trees identified as “Category B” or “Category C” within the approved document “Tree Information to Stage A & B AP/9352/WDC (Ian Keen Limited)”, a scheme for the planting of at least three trees within the landscaped forecourt shall be submitted to the Local Planning Authority.</p> <p>The submitted details shall demonstrate that:</p> <ul style="list-style-type: none"> a) The number and species of the proposed trees would provide substantial replacement canopy cover for the trees removed, and equivalent benefits to the existing trees (in terms of amenity, air quality, biodiversity etc.); b) The specification of the proposed tree pits would ensure sufficient rooting volumes (at least 30 cubic metres per tree unless it is demonstrated that an alternative volume would be acceptable), with a suitable quality growing medium to allow the trees to achieve their canopy potential in maturity; c) The proposed excavation works and any laying of services would be designed to accommodate the required tree pits; d) Suitable specification to ensure successful establishment and survival of the new trees <p>The approved scheme shall be implemented as approved, and the approved trees shall be planted in the earliest available planting season, and at the very latest in the next planting season following first occupation of the development. The approved tree planting shall have a two-year maintenance / watering provision following planting and any new trees which are removed, die, become severely damaged or diseased within five years of planting shall be replaced with the same species or an approved alternative to the satisfaction of the Local Planning Authority within the next planting season.</p> <p>REASON: To ensure that sufficient provision is made within the structural design and excavation works to accommodate appropriate tree pits, in the interest of biodiversity, sustainability, and to ensure that a satisfactory standard of visual amenity is provided and maintained.</p>
7	<p>Bird Boxes (Prior to Occupation)</p>
	<p>CONDITION: Details of bird nesting boxes shall be submitted to and approved in writing by the Local Planning Authority prior the occupation of the hereby approved development. The number and position of bird boxes needs to be determined on site by a qualified ecologist. The details shall include the exact location, specification and design of the habitats.</p> <p>The nesting boxes shall be provided strictly in accordance with the details so approved, installed prior to the first occupation of the building to which they form part or the first use of the space in which they are contained and shall be maintained as such thereafter.</p> <p>REASON: To ensure the development provides the maximum possible provision towards creation of habitats and valuable areas for biodiversity.</p>
8	<p>External Lighting (Prior to Specific Works)</p>

	<p>CONDITION: Full details of external lighting across the site shall be submitted to and approved in writing by the Local Planning Authority prior to the installation of any external lighting.</p> <p>The details shall include the location and full specification of: all lamps; light levels/spill lamps, floodlights, support structures, and hours of operation. The lighting measures shall be carried out strictly in accordance with the details so approved, shall be installed prior to occupation of the development and shall be maintained as such thereafter.</p> <p>REASON: To ensure that any resulting general or security lighting is appropriately located, designed do not adversely impact neighbouring residential amenity and are appropriate to the overall design of the buildings as well as protecting the biodiversity value of the site.</p>
9	<p>*Construction and Environmental Management Plan (Prior to Commencement)</p>
	<p>CONDITION: Notwithstanding the details submitted with the application, an expanded construction management plan shall be submitted to and approved in writing by the Local Planning Authority prior to the commencement of development.</p> <p>The Method of Demolition and Construction Statement shall include details and arrangements regarding:</p> <ul style="list-style-type: none"> a) The notification of neighbours with regard to specific works; b) Advance notification of any access way, pavement, or road closures; c) Details regarding parking, deliveries and storage including details of the routing, loading, off-loading, parking and turning of delivery and construction vehicles and the accommodation of all site operatives', visitors' and construction vehicles during the construction period; d) Details regarding the planned demolition and construction vehicle routes and access to the site; e) Details regarding dust mitigation and measures to prevent the deposit of mud and debris on the public highway. No vehicles shall leave the site until their wheels, chassis and external bodywork have been effectively cleaned and washed free of earth, mud, clay, gravel, stones or any other similar substance; f) Details of waste storage within the site to prevent debris on the surrounding estate and the highway and a scheme for recycling/disposing of waste resulting from demolition and construction works; g) The proposed hours and days of work (with reference to the limitations of noisy work which shall not take place outside the hours of 08.00-18.00 Monday to Friday, 08.00-13.00 on Saturdays, and none on Sundays or Bank Holidays.) h) Details of any proposed external illumination and/or floodlighting during construction; i) Details of measures taken to prevent noise disturbance to surrounding residents; j) Information on access and security measures proposed to prevent security breaches at the existing entrances to the site, to prevent danger or harm to the neighbouring residents, and to avoid harm to neighbour amenity caused by site workers at the entrances to the site; k) Details addressing environmental and amenity impacts (including (but not limited to) noise, air quality, smoke and odour, vibration and TV reception) l) Details of any construction compound including the siting of any temporary site office,

	<p>toilets, skips or any other structure; and</p> <p>m) Details of any further measures taken to limit and mitigate the impact of construction upon the operation of the highway and the amenity of the area.</p> <p>The report shall assess the impacts during the preparation/demolition, excavation and construction phases of the development on the Transport for London controlled Pentonville Road, together with means of mitigating any identified impacts. The report shall also identify other local developments and highways works, including the Kings Cross Gyratory works, and demonstrate how vehicle movements would be planned to avoid clashes and/or highway obstruction on the surrounding roads.</p> <p>No demolition or development shall begin until provision has been made to accommodate all site operatives', visitors' and construction vehicles loading, offloading, parking and turning during the construction period in accordance with the approved details. The demolition and development shall thereafter be carried out in accordance with the details and measures approved in the Method of Construction Statement.</p> <p>The development shall be carried out strictly in accordance with the details so approved and no change therefrom shall take place without the prior written consent of the Local Planning Authority.</p> <p>REASON: In order to secure the safe and efficient operation of the highway network, local residential amenity and to mitigate the impacts of the development.</p>
10	SUDS (Compliance)
	<p>CONDITION: The measures set out in the Sustainable Urban Drainage statement shall be carried out prior to the first occupation of the hereby approved development, unless otherwise approved in writing by the Local Planning</p> <p>REASON: In order to secure sustainable urban drainage, reducing the risk of flooding and to mitigate the impacts of the development.</p>
11	Photovoltaic panels and Air Source Heat Pump (Prior to Occupation)
	<p>CONDITION: Prior to first occupation of the development hereby approved, manufacturers' specifications and a scheme of the detailed designs and layout of the equipment and mechanical systems to be implemented in relation to the proposed PV panels and air source heat pump(s) shall be submitted to and approved in writing by the Local Planning Authority.</p> <p>These details relating to the PV panels shall include but not be limited to: location of PV panels; area of panels; and design (including section drawings showing the angle of panels in-situ, and elevation plans);</p> <p>The solar photovoltaic panels and external plant as approved shall thereafter be installed prior to the first occupation of the development and retained as such permanently thereafter.</p> <p>REASON: In the interests of ensuring that the proposed mechanical plant would achieve</p>

	<p>the performance standards assumed by the approved sustainability statement, to avoid harm to neighbour amenity, to secure sustainable development and to ensure that the resulting appearance and construction of the development is of a high standard of design.</p>
12	<p>Green procurement plan (Prior to specific works)</p> <p>CONDITION: Prior to the commencement of superstructure works, unless otherwise agreed in writing by the Local Planning Authority, a green procurement plan for sourcing the proposed materials shall be submitted to and approved in writing by the Local Planning Authority.</p> <p>REASON: In the interests of securing sustainable development and to minimise the environmental impacts of the development.</p>
13	<p>Landscaping (Prior to specific works)</p> <p>CONDITION: A landscaping scheme shall be submitted to and approved in writing by the Local Planning Authority prior to occupation of the hereby approved development. The landscaping scheme shall include the following details:</p> <ul style="list-style-type: none"> a) a scaled plan showing vegetation to be retained and plants to be planted; b) specification to ensure successful establishment and survival of new planting. c) a schedule detailing sizes, species and numbers of all new trees/plants; d) a biodiversity statement detailing how the landscaping scheme maximises biodiversity; e) existing and proposed underground services and their relationship to both hard and soft landscaping; f) soft plantings: including grass and turf areas, shrub and herbaceous areas; g) topographical survey: including earthworks, ground finishes, top soiling with both conserved and imported topsoil(s), levels, drainage and fall in drain types; h) enclosures: including types, dimensions and treatments of walls, fences, screen walls, barriers, rails, retaining walls and hedges; i) hard landscaping: including surface treatments, permeability of hard surfaces, kerbs, edges, ridge and flexible paving, unit paving, furniture, modular soil systems, steps and if applicable synthetic surfaces; j) a plan showing gradients and step free access suitable for wheelchair users to the entrances of the building; k) any other landscaping features forming part of the scheme. <p>All landscaping in accordance with the approved scheme shall be completed / planted during the first available planting season following the first occupation of the development hereby approved, unless otherwise agreed in writing by the Local Planning Authority. The landscaping and tree planting shall have a two year maintenance / watering provision following planting and any existing tree shown to be retained or trees or shrubs to be planted as part of the approved landscaping scheme which are removed, die, become severely damaged or diseased within five years of completion of the development shall be replaced with the same species or an approved alternative to the satisfaction of the Local Planning Authority within the next planting season.</p> <p>The development shall be carried out strictly in accordance with the details so approved</p>

	<p>and shall be maintained as such thereafter.</p> <p>REASON: In the interest of biodiversity, sustainability, and to ensure that a satisfactory standard of visual amenity is provided and maintained.</p>
14	*Tree Protection (Prior to commencement)
	<p>CONDITION: No works (including site clearance, preparatory work or development) shall take place until a scheme for the appropriate working methods (the Arboricultural Method Statement, AMS) in accordance with British Standard BS 5837 2012 “Trees in Relation to the protection of the street trees to be retained by the development,” has been submitted to and approved in writing by the local planning authority.</p> <p>The details submitted shall include:</p> <ul style="list-style-type: none"> a) Removal of existing structures and hard surfacing; b) Installation of temporary ground protection; c) Excavations; d) Tree works schedule; e) A schedule of specific events requiring input or arboricultural supervision <p>The development shall be carried out in accordance with the approved AMS.</p> <p>REASON: In the interest of biodiversity, sustainability, and to ensure that a satisfactory standard of visual amenity is provided and maintained.</p>
15	Biodiverse Roof (Compliance)
	<p>CONDITION: Notwithstanding the hereby approved details, a biodiverse roof shall be installed on the flat roof over the five storey extension adjacent to Pentonville Road prior to first occupation unless a feasibility assessment is submitted to and approved in writing by the Local Planning Authority.</p> <p>The biodiversity (green/brown) roof(s) shall be:</p> <ul style="list-style-type: none"> a) biodiversity based with extensive substrate base (depth 80-150mm); and b) planted/seeded with an agreed mix of species within the first planting season following the practical completion of the building works (the seed mix shall be focused on wildflower planting, and shall contain no more than a maximum of 25% sedum). <p>The biodiversity (green/brown) roof shall not be used as an amenity or sitting out space of any kind whatsoever and shall only be used in the case of essential maintenance or repair, or escape in case of emergency.</p> <p>The biodiversity roof(s) shall be carried out strictly in accordance with the details so approved and shall be maintained as such thereafter.</p> <p>REASON: To ensure the development provides the maximum possible provision towards creation of habitats and valuable areas for biodiversity</p>

16	Non Road Mobile Machinery (Compliance)
	<p>CONDITION: An inventory of all Non-Road Mobile Machinery (NRMM) must be registered on the NRMM register https://nrmm.london/user-nrmm/register. All NRMM should meet as minimum the Stage IIIA emission criteria of Directive 97/68/EC and its subsequent amendments unless it can be demonstrated that Stage IIIA equipment is not available. All NRMM should be regularly serviced and service logs kept on site for inspection. Records should be kept on site which details proof of emission limits for all equipment.</p> <p>REASON: To comply with the requirements of the NPPF (2012), Policy 7.14 of the London Plan (2016) and to minimise air pollution.</p>
17	Flues / extraction systems (Prior to specific works)
	<p>Details of proposed flues / extraction systems for the units hereby approved shall be submitted to and approved in writing by the Local Planning Authority prior to any works commencing on the unit to which they relate. The filter systems of the approved flue / extraction units shall be regularly maintained and cleaned; and any filters and parts requiring cleaning or replacement shall be easily accessible. The flues/extraction systems shall be carried out strictly in accordance with the details so approved, installed and operational prior to the first occupation of the commercial units to which they relate and maintained as such thereafter.</p> <p>If low level extract vents are proposed, notwithstanding the plans hereby approved, the flue shall be fitted with fine filtration or Electrostatic Precipitation followed by carbon filtration (carbon filters rated with 0.4-0.8 second resistance time) or alternatively fine filtration followed by carbon filtration and by counteractant/neutralising system to achieve the same level as above.</p> <p>The filter systems of the approved flue / extraction units shall be regularly maintained and cleaned; and any filters and parts requiring cleaning or replacement shall be easily accessible.</p> <p>REASON: To avoid unacceptable harm to neighbour amenity and to secure an appropriate residential environment for neighbouring occupiers.</p>
18	BREEAM (Compliance)
	<p>CONDITION: The development shall achieve a BREEAM rating of no less than 'Excellent'.</p> <p>REASON: In the interest of addressing climate change and to secure sustainable development.</p>
19	*Contamination (Prior to commencement)
	<p>Prior to the commencement of development (apart from the roof extension) the following assessment in response to the NPPF and in accordance with CLR11 and BS10175:2011 shall be submitted to and approved in writing by the Local Planning Authority.</p> <p>a) A land contamination investigation.</p>

	<p>Following the agreement to details relating to point a); details of the following works shall be submitted to and approved in writing by the Local Planning Authority prior to any superstructure works commencing on site:</p> <p>b) A programme of any necessary remedial land contamination remediation works arising from the land contamination investigation.</p> <p>The development shall be carried out strictly in accordance with the investigation and any scheme of remedial works so approved and no change therefrom shall take place without the prior written approval of the Local Planning Authority.</p> <p>c) Following completion of measures identified in the approved remediation scheme a verification report, that demonstrates the effectiveness of the remediation carried out, must be produced which is subject to the approval in writing of the Local Planning Authority in accordance with part b).</p> <p>REASON: To avoid unacceptable risk to health arising from contamination.</p>
20	<p>Energy Efficiency – CO2 Reduction (Compliance)</p>
	<p>CONDITION: The energy efficiency measures as outlined within the approved Sustainable Design and Construction Statement shall be installed and operational prior to the first occupation of the development.</p> <p>Should there be any change to the energy efficiency measures within the approved Energy Strategy, a revised Energy Strategy shall be submitted to and agreed in writing by the Local Planning Authority prior to the occupation of the development. This shall include the details of any strategy needed to mitigate poor air quality (such as mechanical ventilation).</p> <p>The final agreed scheme shall be installed and in operation prior to the first occupation of the development.</p> <p>The development shall be carried out strictly in accordance with the details so approved and shall be maintained as such thereafter.</p> <p>REASON: In the interest of addressing climate change and to secure sustainable development.</p>
21	<p>Cycle Parking Provision (Compliance)</p>
	<p>CONDITION: The cycle storage areas and accessibility scooter charging points shown on the hereby approved plans, shall be provided prior to the first occupation of the development, and maintained as such thereafter.</p> <p>REASON: To ensure adequate cycle and accessibility scooter parking is available and easily accessible on site and to promote sustainable modes of transport.</p>
22	<p>Deliveries and Servicing (Compliance)</p>
	<p>CONDITION: Prior to the occupation of the hereby approved development, the waste</p>

	<p>storage areas shown on the approved plans shall be provided. All delivery and servicing must be carried out in accordance with the (draft) Delivery and Servicing Plan hereby as listed in the approved documents on this notice unless otherwise agreed in writing by the Local Planning Authority.</p> <p>REASON: In order to secure highway safety and free flow of traffic, local residential amenity and to mitigate the impacts of the development.</p>
23	SME Workspaces (Compliance)
	<p>A minimum of 100.5sqm of office space (B1a use class) to be provided in two or more units of up to 90sqm in area (with a shared entrance space) shall be provided within the building and maintained at all times.</p> <p>These units shall not be amalgamated nor shall they be incorporated into the remainder of the office floor area.</p> <p>REASON: In the interests of ensuring that the proposed development contributes to a mixed and flexible employment base and specifically supports the ability of small and micro enterprises to find suitable workspace.</p>
24	Roof Gardens (Compliance)
	<p>The roof terraces of the development hereby approved shall not be used except between the hours of 09:00 and 19:00 on any day except in the case of essential maintenance or repair, or escape in case of emergency. No amplified music or licensable activities shall take place on the roof terraces unless otherwise approved in writing by the Local Planning Authority.</p> <p>REASON: To ensure that the amenity of residents is not adversely affected</p>
25	Rainwater Recycling (Prior to specific works)
	<p>CONDITION: Details of a rainwater recycling system shall be submitted to and approved in writing by the Local Planning Authority prior to the commencement of superstructure works, unless a feasibility study is submitted to and approved by the Local Planning Authority demonstrating that it rainwater recycling is not feasible. The rainwater recycling system shall be carried out in accordance with the details so approved, shall be installed and operational prior to the first occupation of the building and shall be retained and maintained as such thereafter.</p> <p>REASON: To ensure the sustainable use of water.</p>
26	Lightwell/Basement Excavation Inspection and Monitoring (Compliance)
	<p>CONDITION: The certifying professional that endorsed the Structural Method Statement (or a suitably qualified person with relevant experience) shall be appointed to inspect, approve and monitor the critical elements of both permanent and temporary basement construction works throughout their duration to ensure compliance with the design approved within the Structural Method Statement and a Building Control body.</p> <p>REASON: To ensure that structural stability has been evaluated by a suitably qualified and</p>

	experienced professional.
--	---------------------------

List of Informatives:

1	S106
	SECTION 106 AGREEMENT You are advised that this permission has been granted subject to a legal agreement under Section 106 of the Town and Country Planning Act 1990.
2	Superstructure
	DEFINITION OF 'SUPERSTRUCTURE' AND 'PRACTICAL COMPLETION' A number of conditions attached to this permission have the time restrictions 'prior to superstructure works commencing on site' and/or 'following practical completion'. The council considers the definition of 'superstructure' as having its normal or dictionary meaning, which is: the part of a building above its foundations. The council considers the definition of 'practical completion' to be: when the work reaches a state of readiness for use or occupation even though there may be outstanding works/matters to be carried out.
3	Community Infrastructure Levy (CIL) (Granting Consent)
	INFORMATIVE: Under the terms of the Planning Act 2008 (as amended) and Community Infrastructure Levy Regulations 2010 (as amended), this development is liable to pay the Mayor of London's Community Infrastructure Levy (CIL). This will be calculated in accordance with the Mayor of London's CIL Charging Schedule 2012. One of the development parties must now assume liability to pay CIL by submitting an Assumption of Liability Notice to the Council at cil@islington.gov.uk . The Council will then issue a Liability Notice setting out the amount of CIL that is payable. Failure to submit a valid Assumption of Liability Notice and Commencement Notice prior to commencement of the development may result in surcharges being imposed. The above forms can be found on the planning portal at: www.planningportal.gov.uk/planning/applications/howtoapply/whattosubmit/cil Pre-Commencement Conditions: These conditions are identified with an 'asterix' * in front of the short description. These conditions are important from a CIL liability perspective as a scheme will not become CIL liable until all of these unidentified pre-commencement conditions have been discharged.
4	Car-Free Development
	INFORMATIVE: (Car-Free Development) All new developments are car free in accordance with Policy CS10 of the Islington Core Strategy 2011. This means that no parking provision will be allowed on site and occupiers will have no ability to obtain car parking permits, except for parking needed to meet the needs of disabled people.

5	Roller Shutters
	<p>The scheme hereby approved does not suggest the installation of external rollershutters to any entrances or ground floor glazed shopfronts. The applicant is advised that the council would consider the installation of external rollershutters to be a material alteration to the scheme and therefore constitute development. Should external rollershutters be proposed a new planning application must be submitted for the council's formal consideration.</p>
6.	Roof top plant
	<p>The applicant is advised that any additional roof top plant not shown on the approved plans will require a separate planning application.</p>
7	Construction works
	<p>Noise from demolition and construction works is subject to control under the Control of Pollution Act 1974. You must carry out any building works that can be heard at the boundary of the site only between 08.00 and 18.00 hours Monday to Friday and 08.00 to 13.00 on Saturday and not at all on Sundays and Public Holidays. You are advised to consult the Pollution Team, Islington Council, 222 Upper Street London N1 1XR (Tel. No. 020 7527 3258 or by email pollution@islington.gov.uk) or seek prior approval under Section 61 of the Act if you anticipate any difficulty in carrying out construction other than within the hours stated above.</p>
8	Thames Water
	<p>Thames Water will aim to provide customers with a minimum pressure of 10m head (approx 1 bar) and a flow rate of 9 litres/minute at the point where it leaves Thames Water pipes. The developer should take account of this minimum pressure in the design of the proposed development.</p> <p>With regard to surface water drainage it is the responsibility of the developer to make proper provision for drainage to ground, water courses or a suitable sewer. In respect of surface water it is recommended that the applicant should ensure that storm flows are attenuated or regulated into the receiving public network through on or off site storage. When it is proposed to connect to a combined public sewer, the site drainage should be separate and combined at the final manhole nearest the boundary. Connections are not permitted for the removal of Ground Water. Where the developer proposes to discharge to a public sewer prior approval from Thames Water Developer Services will be required.</p>
9	Highways Requirements
	<p>Compliance with sections 168 to 175 and of the Highways Act, 1980, relating to "Precautions to be taken in doing certain works in or near streets or highways". This relates, to scaffolding, hoarding and so on. All licenses can be acquired through streetworks@islington.gov.uk. All agreements relating to the above need to be in place</p>

prior to works commencing.

Compliance with section 174 of the Highways Act, 1980 - "Precautions to be taken by persons executing works in streets." Should a company/individual request to work on the public highway a Section 50 license is required. Can be gained through

streetworks@islington.gov.uk. Section 50 license must be agreed prior to any works commencing.

Compliance with section 140A of the Highways Act, 1980 – "Builders skips: charge for occupation of highway. Licenses can be gained through streetworks@islington.gov.uk.

Compliance with sections 59 and 60 of the Highway Act, 1980 – "Recovery by highways authorities etc. of certain expenses incurred in maintaining highways". Haulage route to be agreed with streetworks officer. Contact streetworks@islington.gov.uk.

Joint condition survey required between Islington Council Highways and interested parties before commencement of building works to catalogue condition of streets and drainage gullies. Contact highways.maintenance@islington.gov.uk Approval of highways required and copy of findings and condition survey document to be sent to planning case officer for development in question.

Temporary crossover licenses to be acquired from streetworks@islington.gov.uk. Heavy duty vehicles will not be permitted to access the site unless a temporary heavy duty crossover is in place.

Highways re-instatement costing to be provided to recover expenses incurred for damage to the public highway directly by the build in accordance with sections 131 and 133 of the Highways Act, 1980.

Before works commence on the public highway planning applicant must provide Islington Council's Highways Service with six months notice to meet the requirements of the Traffic Management Act, 2004.

Development will ensure that all new statutory services are complete prior to footway and/or carriageway works commencing.

Works to the public highway will not commence until hoarding around the development has been removed. This is in accordance with current Health and Safety initiatives within contractual agreements with Islington Council's Highways contractors.

Alterations to road markings or parking layouts to be agreed with Islington Council Highways Service. Costs for the alterations of traffic management orders (TMO's) to be borne by developer.

All lighting works to be conducted by Islington Council Highways Lighting. Any proposed changes to lighting layout must meet the approval of Islington Council Highways Lighting. NOTE: All lighting works are to be undertaken by the PFI contractor not a nominee of the developer. Consideration should be taken to protect the existing lighting equipment within and around the development site. Any costs for repairing or replacing damaged equipment as a result of construction works will be the responsibility of the developer, remedial works will be implemented by Islington's public lighting at cost to the developer. Contact streetlights@islington.gov.uk

Any damage or blockages to drainage will be repaired at the cost of the developer. Works to be undertaken by Islington Council Highways Service. Section 100, Highways Act

	<p>1980.</p> <p>Water will not be permitted to flow onto the public highway in accordance with Section 163, Highways Act 1980</p> <p>Public highway footway cross falls will not be permitted to drain water onto private land or private drainage.</p>
10	<p>Fire Safety</p>
	<p>It is recommended that you obtain technical advice regarding compliance with the Building Regulations (and/including matters relating to fire safety and evacuation) prior to any further design work commencing and prior to the selection of materials. In particular, you should seek further guidance regarding the design of the external fabric (including windows) to limit the potential for spread of fire to other buildings. Islington's Building Control team has extensive experience in working with clients on a wide range of projects. Should you wish to discuss your project and how Islington Building Control may best advise you regarding compliance with relevant (building control) regulations, please contact Andrew Marx on 020 7527 2045 or by email on andrew.marx@islington.gov.uk.</p>
11	<p>Superseded Plans</p>
	<p>For clarification, please note that the following documents and submitted with the application are not approved.</p> <p>Superseded Documents Sustainable Design and Construction Statement (EB7, 4th May 2017); Structural Method Statement (Basement Impact Assessment) J3004-S-RP-0003 (Webb Yates Engineers Ltd, 03/04/17); Townscape and Visual Impact Assessment Appendix 3 (Lichfields, May 2017); Ventilation Strategy 3569/VS (Environmental Engineering Partnership, March 2017).</p> <p>Superseded Plans Proposed Lower Ground Floor Plan 2065 A99; Proposed Upper Ground Floor Plan 2065 A100; Proposed First Floor Plan 2065 A101; Proposed Second Floor Plan 2065 A102; Proposed Third Floor Plan 2065 A103; Proposed Fourth Floor Plan 2065 A104; Proposed Fifth Floor Plan 2065 A105; Proposed Sixth Floor Plan 2065 A106; Proposed Seventh Floor Plan 2065 A107; Proposed Roof Plan 2065 A109; Proposed North Elevation 2065 A200; Proposed East Elevation 2065 A202; Proposed South Elevation 2065 A203; Proposed West Elevation 2065 A204; Proposed Roof Extension Details 2065 A2700; Proposed Section AA 2065 A300; Proposed Section BB 2065 A301; Proposed Section CC 2065 A302; Proposed Section DD 2065 A303; Proposed Roof Extension Details 2065 A2700.</p>

APPENDIX 2: RELEVANT POLICIES

This appendix lists all relevant development plan policies and guidance notes relevant to the determination of the planning application.

1 National Guidance

The National Planning Policy Framework 2012 seeks to secure positive growth in a way that effectively balances economic, environmental and social progress for this and future generations. The NPPF is a material consideration and has been taken into account as part of the assessment of these proposals. Since March 2014 planning practice guidance for England has been published online.

2 Development Plan

The Development Plan is comprised of the London Plan 2016, Islington's Core Strategy 2011, Islington's Development Management Policies 2013, the Finsbury Local Plan 2013 and Islington's Site Allocations 2013. The following policies of the Development Plan are considered relevant to this application:

A) The London Plan 2016 Spatial Development Strategy for Greater London

1 Context and strategy

Policy 1.1 Delivering the strategic vision and objectives for London

2 London's places

Policy 2.9 Inner London

Policy 2.10 CAZ Strategic Priorities

Policy 2.11 CAZ Strategic Functions

Policy 2.12 CAZ Local Activities

Policy 2.18 Green Infrastructure

3 London's people

Policy 3.1 Ensuring equal life chances for all

Policy 3.2 Improving health and addressing health inequalities

Policy 3.16 Protection and enhancement of social infrastructure

Policy 3.18 Education facilities

4 London's economy

Policy 4.1 Developing London's economy

Policy 4.2 Offices

Policy 4.3 Mixed use development

Policy 4.10 New and emerging sectors

Policy 4.12 Improving opportunities for all

5 London's response to climate change

Policy 5.1 Climate change mitigation

Policy 5.2 Minimising emissions

Policy 5.3 Sustainable design & construction

Policy 5.4 Retrofitting

Policy 5.5 Decentralised energy networks

Policy 5.6 Decentralised energy in development proposals

Policy 5.7 Renewable energy

Policy 5.8 Innovative energy technologies

Policy 5.9 Overheating and cooling

Policy 5.10 Urban greening

Policy 5.11 Green roofs and development site environs

Policy 5.12 Flood risk management

Policy 5.13 Sustainable drainage

Policy 5.14 Water quality and wastewater infrastructure

Policy 5.15 Water use and supplies

Policy 5.16 Waste net self-sufficiency

Policy 5.17 Waste capacity

Policy 5.18 Construction, excavation and demolition waste

Policy 5.20 Aggregates

Policy 5.21 Contaminated land

6 London's transport

Policy 6.1 Strategic approach

Policy 6.2 Providing public transport capacity and safeguarding land for transport

Policy 6.3 Assessing effects of development on transport capacity
Policy 6.4 Enhancing connectivity
Policy 6.5 Funding Crossrail and other strategically important transport infrastructure
Policy 6.7 Better streets and surface transport
Policy 6.9 Cycling
Policy 6.10 Walking
Policy 6.11 Smoothing traffic flow and tackling congestion
Policy 6.13 Parking

7 London's living places and spaces

Policy 7.1 Lifetime neighbourhoods
Policy 7.2 An inclusive environment
Policy 7.3 Designing out crime
Policy 7.4 Local character
Policy 7.5 Public realm

B) Islington Core Strategy 2011

Spatial Strategy

Policy CS6 (King's Cross)
Policy CS8 (Enhancing Islington's Character)

Strategic Policies

Policy CS9 (Protecting and Enhancing Islington's Built and Historic Environment)
Policy CS10 (Sustainable Design)
Policy CS11 (Waste)
Policy CS12 (Meeting the Housing Challenge)

C) Development Management Policies June 2013

Design and Heritage

DM2.1 Design
DM2.2 Inclusive Design
DM2.3 Heritage
DM2.4 Protected Views
DM2.5 Landmarks

Shops, culture and services

DM4.12 Social and strategic infrastructure and cultural facilities

Policy 7.6 Architecture
Policy 7.8 Heritage assets and archaeology
Policy 7.9 Heritage-led regeneration
Policy 7.11 London View Management Framework
Policy 7.13 Safety, security and resilience to emergency
Policy 7.14 Improving air quality
Policy 7.15 Reducing noise and enhancing soundscapes
Policy 7.18 Protecting local open space and addressing local deficiency
Policy 7.19 Biodiversity and access to nature
Policy 7.21 Trees and woodlands

8 Implementation, monitoring and review

Policy 8.1 Implementation
Policy 8.2 Planning obligations
Policy 8.3 Community infrastructure levy

Policy CS13 (Employment Spaces)
Policy CS 15 (Open Space and Green Infrastructure)

Infrastructure and Implementation

Policy CS 18 (Delivery and Infrastructure)
Policy CS 19 (Health Impact Assessments)
Policy CS 20 (Partnership Working

Employment

DM5.1 New business floorspace
DM5.2 Loss of existing business floorspace
DM5.4 Size and affordability of workspace

Health and open space

DM6.1 Healthy development
DM6.2 New and improved public open space
DM6.5 Landscaping, trees and biodiversity
DM6.6 Flood prevention

Energy and Environmental Standards

- DM7.1 Sustainable design and construction
- DM7.3 Decentralised energy networks
- DM7.4 Sustainable design standards
- DM7.5 Heating and cooling

Transport

- DM8.1 Movement hierarchy
- DM8.2 Managing transport impacts
- DM8.3 Public transport

- DM8.4 Walking and cycling
- DM8.5 Vehicle parking
- DM8.6 Delivery and servicing for new developments

Infrastructure

- DM9.1 Infrastructure
- DM9.2 Planning obligations
- DM9.3 Implementation

3 Designations

The site has the following designations under the London Plan 2016, Islington Core Strategy 2011, Development Management Policies 2013 and Site Allocations June 2013.

- Central Activities Zone (CAZ)
- Article 4 Flexible Uses (removes PD rights which allow the change from A1 (shops), B1 (offices) and D1 (community uses) to temporary 'flexible uses')
- Core Strategy Key Area: Kings Cross and Pentonville Road
- Licensing Cumulative Impact Area 5 (Clerkenwell)
- Boundary of London Plan Protected Vista: Kenwood viewing gazebo to St Paul's Cathedral

4 Supplementary Planning Guidance (SPG) / Document (SPD)

The following SPGs and/or SPDs are relevant:

Islington Local Development Plan

- Environmental Design SPD
- Inclusive Design in Islington SPD
- Inclusive Landscape Design SPD
- Planning Obligations (Section 106) SPD
- Streetbook SPD
- Urban Design

London Plan

- Accessible London: Achieving an Inclusive Environment SPG
- The Control of Dust and Emissions During Construction and Demolition SPG
- Planning for Equality & Diversity SPG
- Shaping Neighbourhoods – Character and Context SPG
- Sustainable Design and Construction SPG
- London Planning Statement (May 2014)
- Central Activities Zone SPG
- London View Management Framework SPG
- Use of Planning Obligations in the Funding of Crossrail, and the Mayoral Community Infrastructure Levy SPG

CONFIDENTIAL

ATT: Zoe Simmonds
Nathaniel Lichfield & Partners,
14 Regent's Wharf,
All Saints Street,
London N1 9RL

Planning Service
Planning and Development
PO Box 333
222 Upper Street
London
N1 1YA

T 020 7527 2389
F 020 7527 2731
E Luciana.grave@islington.gov.uk
W www.islington.gov.uk

Our ref: DRP/111

Date: 6 February 2017

Dear Zoe Simmonds,

ISLINGTON DESIGN REVIEW PANEL

RE: York House, 207-221 Pentonville Road, London N1 9UZ (pre-application ref. Q2015/4128/MJR)

Thank you for attending Islington's Design Review Panel meeting on 16 January 2017 for the first review of the above scheme. The proposed scheme under consideration is for the refurbishment and extensions to the existing building comprising the refurbishment of existing elevations including replacement, enlarged, and additional fenestration, 5 storey front extension, infilling of the rear basement/undercroft area, and additional lightwells to the front elevation, 2 additional storeys at roof level and an additional residential or office building to the rear, attached to 4 Lorenzo Street (officer's description).

Review Process

The Design Review Panel provides expert impartial design advice following the 10 key principles of design review established by Design Council/CABE. The scheme was reviewed by Richard Portchmouth (Chair), Neil Williamson, George Saumarez Smith, Thomas Lefevre, Sarah Jackson and Charles Thomson on 16 January 2017 including a morning site visit and presentation from the design team followed by a question and answer session and deliberations at the offices of the London Borough of Islington. The views expressed below are a reflection of the Panel's discussions as an independent advisory body to the Council.

Panel's observations

The Panel was generally supportive of the design team's approach for the site and of the sympathetic refurbishment of the existing building. Panel members welcomed the creation of a new front entrance bay to Pentonville Road and also providing a more active frontage across the main front elevation at street level. The Panel made the following comments.

New entrance extension

Panel members supported the principle of a new entrance extension to the front of the building and felt that this would greatly improve the entrance of the building and better use the existing forecourt space. The Panel agreed that providing entrances to the two sides of the extension would make sense for pedestrians visiting the building and arriving from either the east or west down Pentonville Road. However, panel members raised some

concerns about the articulation of the front elevation of the extension which may appear as a blank or inactive façade fronting the street. Some panel members felt that this could be addressed by allowing further views or glimpses through to the entrance hall from the front. The Panel raised some concerns with regard to the elevational treatment, using a glazed wall with brickwork in front particularly its impact in terms of the energy efficiency of the building and the robustness of the ground floor.

Roof extension

Panel members did not object to the roof extension to the existing office building in principle, but felt that on drawings shown in the presentation the addition appeared to have been played down. As such it was not possible to fully assess the impact of the roof extension on the building or its design in detail. Some panel members raised concerns over the stepped nature of the two additional storeys and felt that this could be improved. The Panel felt that the roof extension should be treated quite simply so as to not introduce too many new forms and materials to the building.

Alterations to existing building

Panel members generally supported the addition of glazing to the east elevation, but were less convinced by the arrangement of the proposed glazing to the south elevation. The Panel felt that the design team were handling the upgrading of the existing building sensitively and were impressed with their relatively 'light touch' approach. Some panel members raised concerns over the floor to ceiling windows and the potential for clutter behind the glass and agreed that this should be designed out through the office layouts internally.

New building on Lorenzo Street

The Panel did not feel that the new building adjacent to the two locally listed buildings on Lorenzo Street should be a replica of what is next door, particularly with the problems with accessibility etc. They felt that it would be better as a distinct building, which mediates between the architecture of the locally listed buildings and the main office building fronting Pentonville Road. Panel members also commented that a contemporary building may look interesting, but it would need to relate to the scale and morphology of the domestic houses, not the office building.

Views

The Panel felt that a number of verified views would be necessary in order to assess the proposals and the impact of the extensions on the street scene and adjacent conservation area. They commented that winter photographs should be provided from all potential viewpoints in order to give an objective basis for deciding which viewpoints should be selected for detailed assessment.

Trees

Panel members raised concerns that the street trees were proposed to be removed, but these concerns were somewhat abated following confirmation from the planning case officer that consent was very unlikely to be granted for their removal. The Panel did, however, agree that the replacement of the existing trees on the main road frontage with better quality trees of more long-lived species as part of the wider improvement of the frontage would be beneficial.

Environmental performance

Panel members felt that maximising the environmental performance of the building and reducing carbon emissions as far as possible should form a key part of the proposals and

encouraged the design team to explore the options in order to improve the building's performance as far as possible.

Summary

The Panel were very supportive of the refurbishment of the existing building and welcomed the proposed enhancements to the building and of the design team's approach. They raised some concerns over the height and form of the roof extension, but advised that further information was required in order to properly assess this. The Panel also welcomed the new entrance extension to Pentonville Road, but felt that the front elevation of the extension required further thought. Panel members were encouraged by the aspirations of the design team and felt that when finished the building should be an exemplar of adaptive reuse.

Thank you for consulting Islington's Design Review Panel. If there is any point that requires clarification please do not hesitate to contact me and I will be happy to seek further advice from the Panel.

Confidentiality

Please note that since the scheme is at pre-application stage, the advice contained in this letter is provided in confidence. However, should this scheme become the subject of a planning application, the views expressed in this letter may become public and will be taken into account by the Council in the assessment of the proposal and determination of the application.

Yours sincerely,



Luciana Grave

Design Review Panel Coordinator
Design & Conservation Team Manager

APPENDIX 4: DESIGN REVIEW PANEL (2)



ATT: Zoe Simmonds
Nathaniel Lichfield & Partners
14 Regent's Wharf
All Saints Street
London
N1 9RL

Planning Service
Planning and Development
PO Box 333
222 Upper Street
London
N1 1YA

T 020 7527 2389
F 020 7527 2731
E Luciana.grave@islington.gov.uk
W www.islington.gov.uk

Our ref: DRP/127

Date: 30 June 2017

Dear Zoe Simmonds,

ISLINGTON DESIGN REVIEW PANEL

**RE: York House, 207-221 Pentonville Road, London N1 9UZ
(planning application ref. P2017/1790/FUL)**

Thank you for attending Islington's Design Review Panel meeting on 13 June 2017 for a second review of the above scheme. The proposed scheme under consideration is for the addition of two storeys to the existing building (including lift overrun and 1.35m increase to parapet height); a five storey front entrance extension; and a part 1, part 2 storey rear extension at upper and lower ground floor levels including the infilling of the rear undercroft area, to provide 1228 sqm (GIA) of additional office space (Use Class B1). Internal and external refurbishment works to provide ancillary gym and cafe areas; new fenestration; a mural on the south elevation; construction of a new front elevation lightwell; erection of new cycle and bin stores in refurbished service yard; hard and soft landscaping (including replacement of trees); roof level plant; and associated works (officer's description).

Review Process

The Design Review Panel provides expert impartial design advice following the 10 key principles of design review established by Design Council/CABE. The scheme was reviewed by Richard Portchmouth (Chair), Neil Williamson, George Saumarez Smith, Sarah Jackson and Charles Thomson on 13 June 2017 including a presentation from the design team followed by a question and answer session and deliberations at the offices of the London Borough of Islington. There was no site visit as this was a second review. The views expressed below are a reflection of the Panel's discussions as an independent advisory body to the Council.

Panel's observations

The Panel welcomed the opportunity to comment on the scheme for a second time and remained supportive of the design team's approach to the site. Comments made by panel members predominantly related to fine-tuning the design of the new additions to the building, notwithstanding the contrasting views expressed with regard to the impact of the extra height and bulk at roof level. The Panel agreed with the strategy of not including a residential element on Lorenzo Street owing to the extremely confined site and lack of good amenity. The Panel were informed that a financial contribution would be made in lieu.

Front addition

The Panel commended the design strength and abstract quality of the brick lattice structure, and welcomed the positive use of redundant space at the front of the building which represents an enhancement of the public realm.

Panel members expressed a range of differing views on how best to reconcile the desire to give the street fronting elevation more of an active frontage, incorporating human-scale elements, without compromising the concept and overall composition. Whilst some panel members considered the introduction of three large openings to be a successful response, others were mindful that these openings would make the transparent nature of the lattice brickwork more difficult to appreciate. In light of the diversity of the comments, the Panel felt that further experimentation could result in a solution that satisfied all concerns without compromising the design. This could be the creation of a more transparent lattice which utilized larger or doubled spaced openings in the lattice pattern, rather than punched openings or portals. The Panel also felt that a more convincing argument needed to be developed, demonstrating that what is ultimately proposed is the correct approach, clearly showing how the chosen arrangement had been settled upon and why others had been ruled out.

Roof addition

Panel members welcomed the revision of this element in accordance with previous advice – in that it has been simplified so that it reads as a single entity. The use of metal cladding in place of brick was also welcomed by the Panel, as it is considered to help to disassociate the extension from the main body of the building, appearing as a much lighter weight structure. However, some Panel members raised significant concern over the extra bulk now present in long views down Pentonville Road and from within the Conservation Area. Nevertheless, the majority of the Panel considered the height not unacceptable, and perhaps necessary in enabling the quality of the design to be recognized and noticed. Although the two points of view expressed are somewhat conflicting, the Panel regarded them both as equally valid considerations. Consequently, in the view of the Panel, the acceptability of the size of the roof addition will ultimately rest upon the quality of the design – great emphasis was therefore placed on the importance of showing that much care and detail has gone into the final design, in addition to a greater range of verified views, to ensure that decision takers are able to make as informed a judgement as possible. The Panel suggested that an in situ mock up sample panel of the attic storey would be of benefit in assessing the design quality.

Rear elevation

The Panel discussed the proposal of introducing some form of public art to animate the large blank southern elevation and generally supported the idea which they believed was more in keeping with the existing character of the building than introducing an ad hoc window pattern. However, the Panel considered that a degree of control over what is put there to be absolutely essential, stating that the acceptability of this intervention rests entirely upon the quality and contextual appropriateness of the artwork. Panel members suggested that it should be of the same stature as the existing Geoffrey Clarke sculpture and perhaps similar in nature (either abstract, sculptural or both), which would help to provide an element of control. Some Panel members suggested that the Design Review Panel could have further input in drawing up selection criteria or judging entries.

Servicing

Panel members queried the space available for servicing the building at the rear, particularly regarding turning space for vehicles, and recommended that this was double checked.

Summary

The Panel's comments are framed with an appreciation of the ambition and careful thought given to markedly improve what is currently a very ordinary building – the overall response was very positive. Ultimately, the Panel encouraged a stronger rationale to be developed to show that the treatment of the proposed front and roof extensions represent the best possible arrangements so that the true potential for the project to become an exemplary design is realised.

Thank you for consulting Islington's Design Review Panel. If there is any point that requires clarification please do not hesitate to contact me and I will be happy to seek further advice from the Panel.

Confidentiality

Please note that since the scheme is at planning application stage, the views expressed in this letter may become public and will be taken into account by the council in the assessment of the proposal and determination of the application.

Yours sincerely,

A handwritten signature in black ink, appearing to read 'Lucy', enclosed in a thin black rectangular box.

Luciana Grave

Design Review Panel Coordinator
Design & Conservation Team Manager